

**FINAL DRAFT
VILLAGE OF SUFFERN
CONSERVATION DISTRICT PLAN**

Prepared by:

**ROBERT GENESLAW CO.
2 Executive Boulevard
Suffern, NY 10901**

Prepared for:

**Village of Suffern
51 Washington Avenue
Suffern, NY 10901**

MARCH 1995

"Nature has lavished rich gifts on Suffern. Beautiful scenery. Delightful climate. Wonderful soil. Suffern enterprise has done well its part...let all of us who call Suffern home - all whose hearts civic pride beats high..resolve to work more closely together for greater and greater accomplishments. Thus not only all physical roads, but all roads of Knowledge, Beauty, Social Life, Manufacture and Commercial Supremacy Lead to Suffern."

- "All Roads Lead to Suffern" Celebration Brochure, 1921

ACKNOWLEDGEMENTS

Suffern Conservation District Committee

Jack Keough, Chairman
Fred Gentile
Howard Glinsky
Richard Hoffman
Craig Long
Kate O'Hanlon
Gardner F. Watts

Village Board of Trustees

Honorable George Parness, Mayor
Andrew Haggarty
Ellen C. Jaffe
Jack Rosenberg
Paul Traub

Cornell Cooperative Extension

Mr. Paul Trader

TABLE OF CONTENTS

	<u>Page No.</u>
INTRODUCTION	iv
I. CONSERVATION DISTRICT STUDY AREA BOUNDARIES	I-1
II. EXISTING CONDITIONS	II-1
A. LAND USE	II-1
B. ZONING	II-6
C. PHYSICAL ATTRIBUTES OF EXISTING PROPERTIES AND BUILDINGS	II-12
D. STREETScape	II-15
1. Signs	II-15
2. Street Furniture	II-15
3. Street Trees and Plantings	II-16
4. Lighting	II-18
5. Plazas	II-20
6. Other Visual Features	II-21
E. CIRCULATION AND PARKING	II-22
1. Pedestrian Circulation	II-22
2. Vehicular Circulation	II-24
3. Curbing and Curb Cuts	II-26
4. Parking	II-26
F. SECURITY	II-28
III. EXAMPLES OF NEARBY VILLAGE STREETScapeS	III-1
A. RIDGEWOOD, NEW JERSEY	III-1
B. WESTWOOD, NEW JERSEY	III-3
C. HILLSDALE, NEW JERSEY	III-4
D. PARK RIDGE, NEW JERSEY	III-5
E. PEARL RIVER, NEW YORK	III-5
F. ALLENDALE, NEW JERSEY	III-5

TABLE OF CONTENTS (cont.)

	<u>Page No.</u>
IV. RECOMMENDATIONS	IV-1
A. LAND USE	IV-1
B. ZONING	IV-1
C. PHYSICAL ATTRIBUTES OF EXISTING PROPERTIES AND STRUCTURES	IV-1
D. STREETScape	IV-2
E. CIRCULATION AND PARKING	IV-3
F. SECURITY	IV-4
 V. CONSERVATION DISTRICT PLAN	 V-1
A. CONSERVATION DISTRICT OVERLAY AND BOUNDARIES	V-1
B. LAND USE	V-2
1. Conversion of Residential Uses to Commercial Uses	V-2
2. Conversion of Auto-related Uses along Orange Avenue	V-2
C. ZONING	V-2
D. PROPERTIES AND BUILDINGS	V-3
1. Facade Materials	V-3
2. Roofline	V-4
3. Architectural Details	V-4
4. Window Treatment	V-4
5. Building Entry	V-4
6. Building Height	V-4
E. STREETScape	V-5
1. Signs	V-5
2. Street Furniture	V-7
3. Street Trees and Plantings	V-8
4. Lighting	V-13
5. Plazas	V-14
6. Other Visual Features	V-14
F. CIRCULATION AND PARKING	V-15
1. Pedestrian Circulation	V-15
2. Vehicular Circulation	V-16
3. Curbing and Curb Cuts	V-16
4. Parking	V-16

TABLE OF CONTENTS (cont.)

	<u>Page No.</u>
G. SECURITY	V-16
VI. IMPLEMENTATION	VI-1
A. ADOPTION OF THE CONSERVATION DISTRICT PLAN AS AN ELEMENT OF THE MASTER PLAN	VI-1
B. CONSERVATION DISTRICT OVERLAY ZONING	VI-1
C. BUSINESS IMPROVEMENT DISTRICT	VI-2
D. CAPITAL IMPROVEMENT PROGRAM	VI-4
E. LOCAL HISTORIC DISTRICT	VI-5
VII. FUNDING	VII-1
A. NEW YORK STATE URBAN DEVELOPMENT CORPORATION	VII-1
B. NYS COUNCIL ON THE ARTS	VII-2
C. PRESERVATION LEAGUE	VII-3
D. NATIONAL SMALL BUSINESS ADMINISTRATION TREE PLANTING PROGRAM	VII-3
E. NEW YORK MAIN STREET ALLIANCE	VII-3
F. NEW YORK STATE DEPT. OF TRANSPORTATION	VII-4
G. LOCAL FUNDING	VII-4
1. Suffern Parking Authority	VII-4
2. Suffern Chamber of Commerce	VII-4
3. Community Development Block Grants	VII-5

APPENDICES

- A. PROPERTY AND STRUCTURES INVENTORY
- B. CORNELL COOPERATIVE EXTENSION STREET TREE INVENTORY
- C. NYSDOT PRELIMINARY PLAN FOR IMPROVEMENTS TO
WAYNE AVE./ORANGE AVE. INTERSECTION

LIST OF FIGURES

	<u>Follows Page No.</u>
1. Property Identification Map	I-1
2. Existing Land Use: Ground Floor Level	II-2
3. Existing Land Use: Basement and Upper Floor Levels	II-2
4. Zoning	II-6
5. Buildings of Interest	II-14
6. Streetscape Features	II-16
7. Circulation and Parking	II-20
8. Proposed Conservation District Overlay Boundaries	V-2
9. Photos of Important Design Features	V-5
10. Comparison of Design Details	V-5
11. Waste Receptacle Detail	V-7
12. Protective Devices for Street Trees	V-8
13. Streetscape Plan	V-12
14. Decorative Street Lamps	V-13

LIST OF TABLES

	<u>Page No.</u>
1. Permitted and Special Permit Uses within the Study Area	II-7
2. Bulk Regulations Applicable to Zoning Districts Within the Study Area	II-8
3. Tree Planting Plan	V-10

INTRODUCTION

The Village of Suffern's location at the foot of the Ramapo Pass, a major transportation hub, has influenced the growth and development of the community since its inception in 1773, when John Suffern purchased his property at this "point of the mountains." Nestled at the foothills of the Ramapo Mountains, Suffern would first begin as a farming community, but begin its conversion to a major commercial crossroads in 1841 with the completion of the New York & Erie Railroad. Construction of the Paterson and Ramapo Railroad from Jersey City would follow and be completed in 1848. Around this time, grandsons of the community's founder would begin a general store at the junction of Wayne and Orange Avenues. The Mountain House, the village's first hotel, would follow next door. Dwight Baker, a settler who arrived in the Village in 1866, would open a general store, wheelwright, carriage shop and lumberyard along Orange Avenue at the foot of Lafayette. Other shops followed. Around the 1860s, Suffern became a popular summer haven with its numerous hotels and boardinghouses.

By 1884, a total of 20 stores were clustered near the depot on Orange, Wayne and lower Lafayette Avenue. The Village incorporated in 1896, and the "commercial hub" expanded. Expansion of the year-round residential community supported further commercial development. Suffern's growth and commercial success was evidenced by the creation of its own local bank, the Suffern National Bank, in 1901. A sign of progress, Lafayette Avenue was paved in 1921. The 1920s resulted in the greatest and most rapid expansion in the commercial district. This historic expansion is still evidenced today in the many buildings lining Lafayette Avenue that date to this time period. Behind 50s, 60s, 70s and 80s facades, some buildings date to the turn of the century.

Today, Suffern is a well-established community whose downtown remains commercially viable even during a time when strip commercial development and malls continue to erode other "main street" communities. The growth of suburban neighbors, including Mahwah to the south, and the completion of I-287, pose opportunities to capture future commercial demand. However, Suffern competes with the auto convenience of nearby strip centers, and must work harder to benefit from these opportunities. The Village's ability to capture future demand will depend upon the present and future quality of its downtown environment, measured by factors including:

- the mix of retail and service commercial businesses that, while specialized or focused to local needs, derive benefits from being "in the mix";
- adequate parking, perceived or reasonably distributed and convenient;

- the visual image of the downtown district, reflected in its buildings and streetscape, i.e., lighting, trees, signage, and street furniture; and
- perceived security of visitors utilizing the services available downtown.

Fortunately, Suffern has a special character, something which new centers do not offer - a rich history and urban fabric which weaves residential, commercial, office and similar uses along one "main street." Suffern must continue its activities in preserving and enhancing the features of its downtown that make it unique and lend the community its sense of place.

The conservation district plan seeks to preserve and enhance this sense of place by identifying the positive qualities of Suffern's downtown and its approaches, providing guidelines for its improvement, and defining implementation measures and funding sources that will assist the Village in effectuating the recommendations of the Plan.

I. CONSERVATION DISTRICT STUDY AREA BOUNDARIES

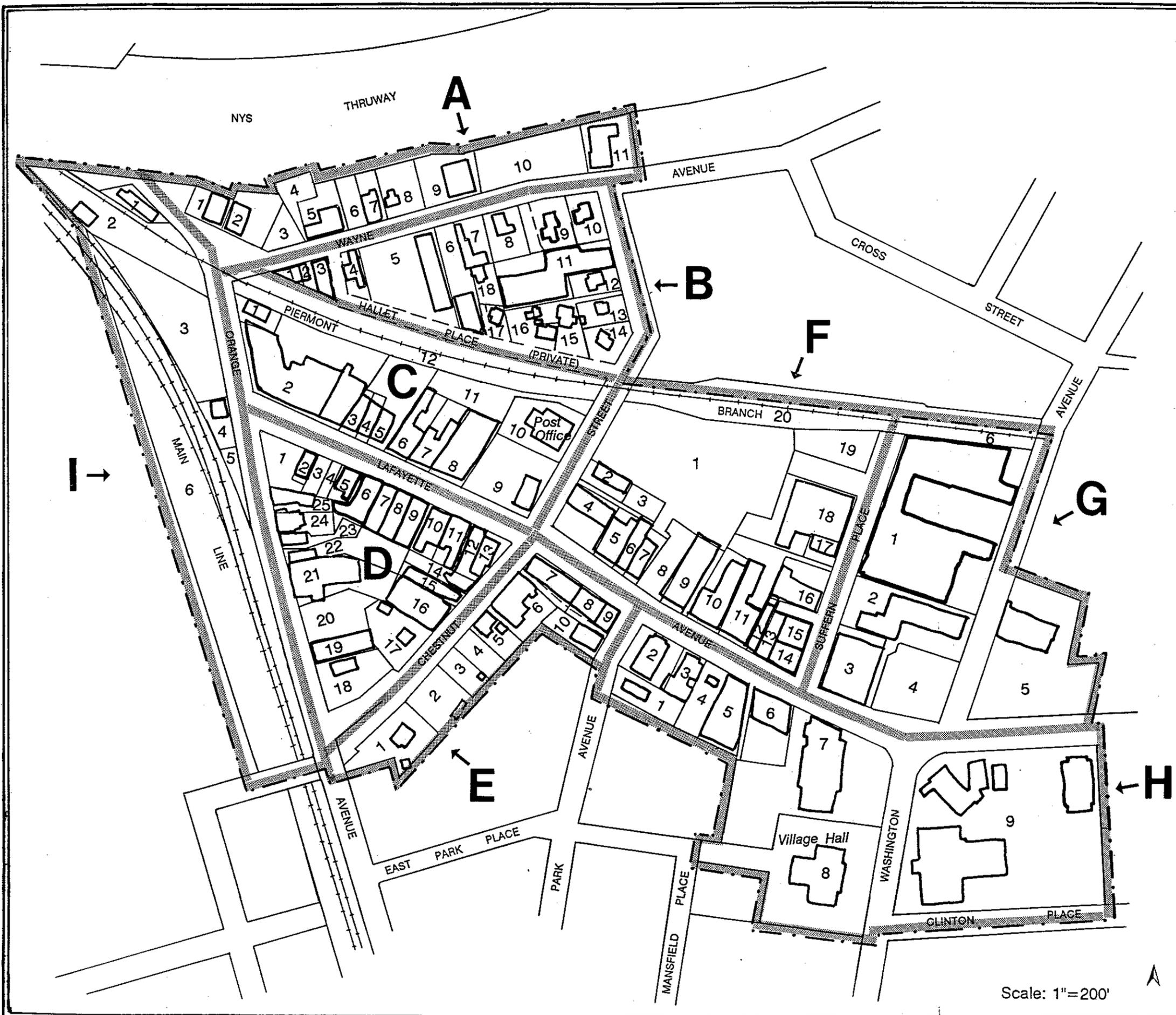
Figure 1 illustrates the study area boundary. This boundary was established based on the recommendations of the planning consultant, with input from the Conservation District Committee. The study area's boundary was established, recognizing the need to include several key locations, including Sacred Heart Church and the war monument, the central business district, and the major visual and transportation points of entry into the study area, including the intersection of Wayne and Orange Avenues, and Lafayette and Washington Avenues. The study area's northerly boundary begins at a point northwest of Orange Avenue's intersection with Wayne Avenue and immediately south of the New York State Thruway overpass over the existing rail right-of-way, and Orange Avenue. Heading from this point in a northeasterly direction, the study area includes properties fronting on both sides of Wayne Avenue to the avenue's intersection with Chestnut Street.

The boundary then travels in a southerly direction, including properties on the westerly side of Chestnut Street to the point where the Piermont Branch crosses Chestnut Street. From this point, the study area boundary heads in an easterly direction following the rail right-of-way until it reaches Washington Avenue. The boundary heads in a southerly direction along the centerline of Washington Avenue until it changes in an easterly direction to encompass an existing office property and parking lot at the intersection of Washington Avenue and Lafayette Avenue. The boundary continues in a southerly direction to encompass the Sacred Heart Church property. The study area boundary then travels in a westerly direction along the centerline of Clinton Place, crosses Washington Avenue, and follows the southerly boundary of the Village Hall property. It then follows in a northerly direction to encompass the municipal parking lot behind the Miranda Building, and then in a westerly direction, along the southern perimeter of properties fronting on Lafayette Avenue. The study area boundary crosses Park Place, where it then heads in a southwesterly direction to encompass properties fronting along Chestnut Street. The study area boundary crosses over Orange Avenue in the vicinity of the Chestnut Street overpass, and then travels in a northwesterly direction to encompass the rail right-of-way up to the New York State Thruway overpass.

The primary visual entrypoints into the central business district include the intersection of Wayne Avenue and Orange Avenues, Chestnut Street intersection with Wayne Avenue, the intersection of Washington and Lafayette Avenue, and Orange Avenue in the vicinity of the Chestnut Street overpass.

Figure No. 1
**PROPERTY
 IDENTIFICATION
 MAP**

Note:
 Refer to Appendix A for
 corresponding tax map
 and parcel numbers.



VILLAGE OF SUFFERN
 * CONSERVATION DISTRICT *
 STUDY

II. EXISTING CONDITIONS

The purpose of this section is to provide an overview of the elements that characterize the study area's environment as Suffern's commercial and civic center. These elements include land use, existing zoning, a physical description of existing buildings and properties, the streetscape and security. Once an identification of existing conditions is made, these conditions can be compared to elements contained within other "village-like" communities to assess what other issues affect the study area, and what opportunities are available. Recommendations are made, a Streetscape Plan devised, and implementation strategies are presented to effectuate the Plan.

Appendix A provides a summary of features for all the properties and structures within the study area in matrix form. The information contained in this matrix has been summarized in the following sections. **Figure 1** provides the property identification numbers that correspond to the properties within the study area referenced in **Appendix A**.

A. LAND USE

Existing land use within the conservation district study area was inventoried to provide answers to the following questions:

- Do the types of land uses support daytime or evening patrons?
- Is there a concentration of existing land uses that are important to retain in the study area, and are they concentrated in particular portions of the study area?
- Have existing buildings been altered to support uses that generate activity in the study area, and are they pedestrian-friendly?
- Does existing zoning support and encourage the types of uses appropriate to the study area?

The following discussion provides a summary of the findings of the land use inventory.

1. Land Use Categories

Land uses within the downtown district were identified in accordance with the following land use categories:

- Detached Residential
- Other Residential
- Retail/Service Commercial
- Eating and Drinking
- Office
- Bank (including an identification of drive-thrus)
- Auto-Related
- Non-retail Commercial
- Community Facilities and Services
- Parking (including an identification of private and municipal lots)
- Vacant

The inventory also presents an identification of uses according to floor level, i.e., 1st floor uses, 2nd floor uses, etc. **Figures 2 and 3** illustrate land use patterns within the conservation district study area.

Detached residential includes all structures that are utilized solely for residential purposes, including single family, two-family, and multi-family dwellings. The "other residential" category identifies apartments found in structures containing non-residential uses.

"Retail/service commercial" includes uses oriented to direct sales and services to a consumer, including clothing stores, grocery, instruction, furniture sales, hardware sales, pharmacy, dry cleaner, and similar uses. "Fast-food" establishments that are carry-out only, with no sitting accommodations, are included in this category.

"Eating and drinking" includes uses that provide food and beverage for consumption on the premises, and may also have the capacity for carry-out orders. Pubs, cafes, coffee shops, and restaurants are included in this category.

"Office" includes uses where a service is being performed, there is generally no retail sale of goods, e.g., engineering office, attorney, real estate office. "Banks" include all financial depository institutions. Banks supporting a "drive-thru" are identified.

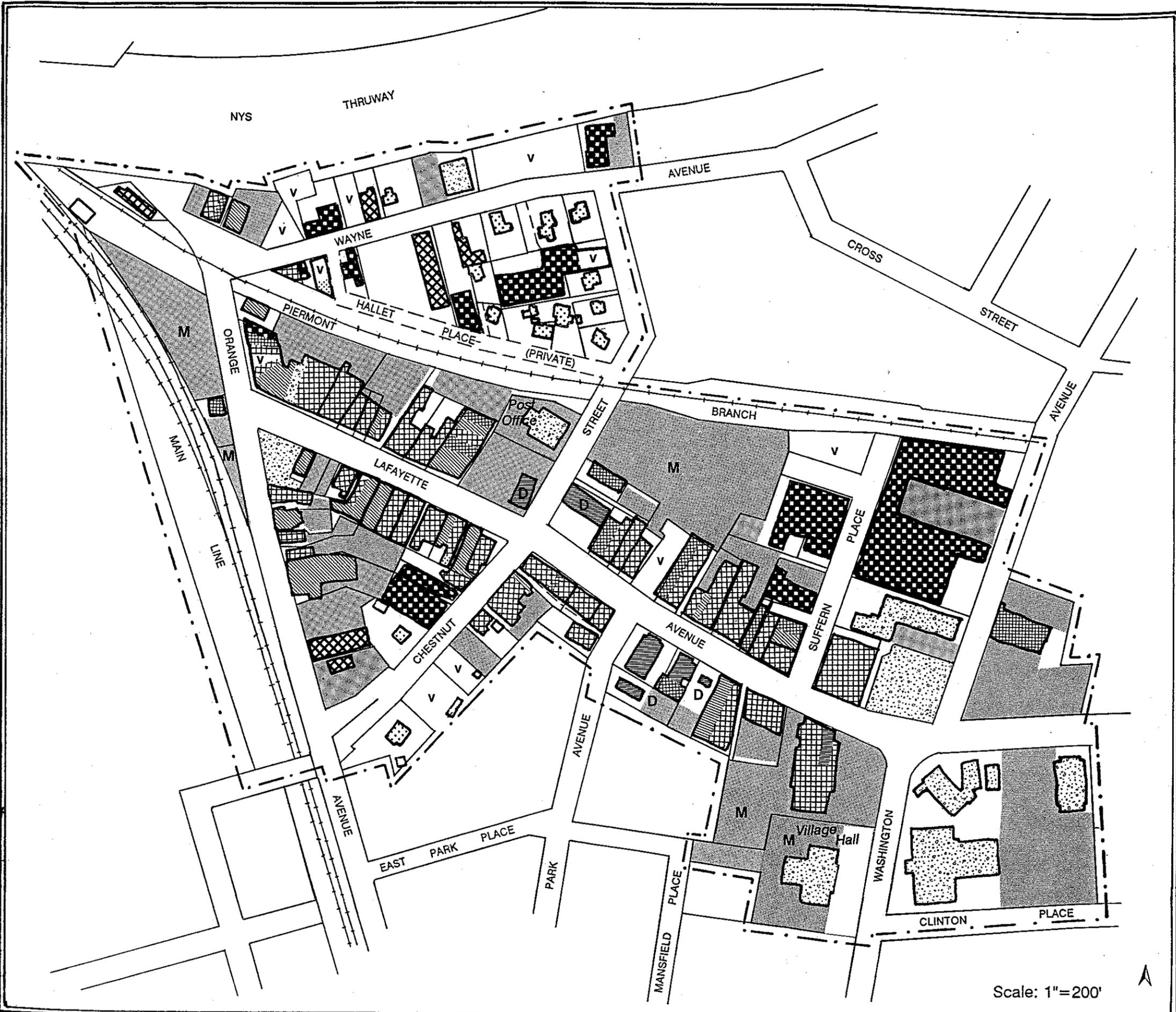
The "auto-related" category includes uses that provide for the servicing, repair and maintenance of motor vehicles.

The "heavy commercial/light industrial" category identifies a range of non-residential uses, including warehousing and storage, light industrial, plumbing supply, metal fabrication, and similar uses.

Figure No. 2
**EXISTING LAND USE:
 GROUND FLOOR LEVEL**

LEGEND

-  **Detached Residential**
-  **Other Residential**
-  **Retail/Service Commercial**
-  **Professional Office**
-  **Eating & Drinking**
-  **Bank (D=Drive Through)**
-  **Non-Retail Commercial**
-  **Auto Related**
-  **Community Facilities**
-  **Parking (M=Municipal)**
-  **Vacant**

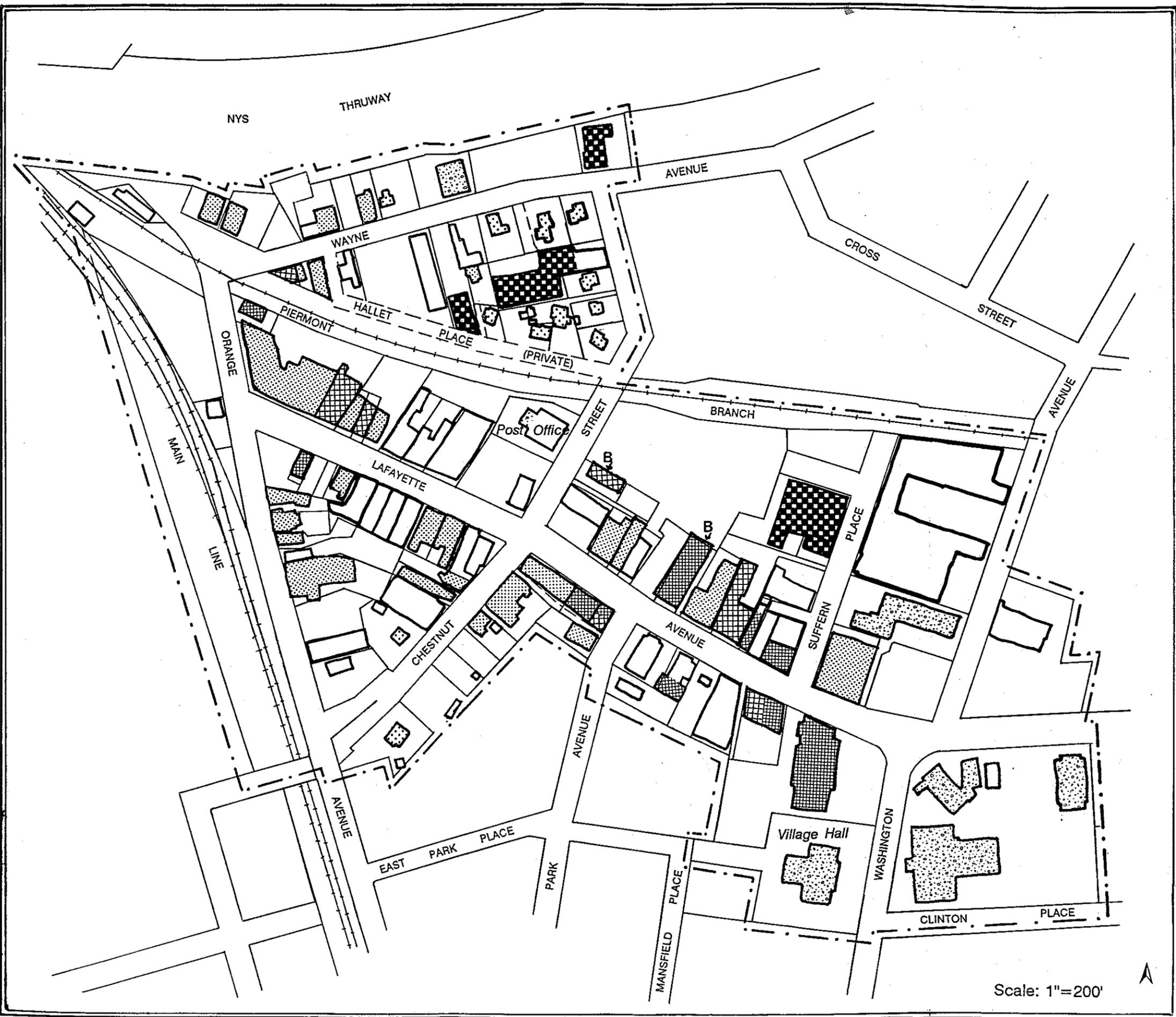


VILLAGE OF SUFFERN
 * **CONSERVATION DISTRICT** *
STUDY

Figure No. 3
EXISTING LAND USE:
BASEMENT &
UPPER FLOOR LEVELS

LEGEND

-  **Detached Residential**
-  **Other Residential**
-  **Retail/Service Commercial**
-  **Professional Office**
-  **Eating & Drinking**
-  **Bank (D=Drive Through)**
-  **Non-Retail Commercial**
-  **Auto Related**
-  **Community Facilities**
-  **Parking (M=Municipal)**
-  **Vacant**



VILLAGE OF SUFFERN
 * **CONSERVATION DISTRICT** *
STUDY

"Community facilities and services" describes public and quasi-public uses, e.g., village hall, churches, synagogues, parks, etc.

The category "parking lots" includes all portions of properties, or entire properties, used for the temporary parking of motor vehicles.

The "vacant" category identifies properties that have no structures, and there is no use of the land occurring.

2. Land Use Patterns

The study area includes a wide range of services and uses meeting the needs of different market segments. **Figures 2 and 3** graphically illustrate the land use pattern within the conservation district study area.

Detached residential uses are found at the periphery of the study area, at either end of Chestnut Street, and along Wayne Avenue, east of its intersection with Orange Avenue. In addition, a concentration of detached residential uses may also be found along Hallett Place. The detached residential structures include single family, two-family, and multi-family dwellings. These uses, given their lower density character, strengthen the sense of transition and entry into the business district. Residents are primarily in the study area during the evening hours, as well as weekends. A portion of the property containing the Park Place high rise apartment complexes fronts onto Chestnut Street and is identified as "Detached Residential". This portion of the property contains an accessory swimming pool facility.

The "other residential" category includes apartments located on the second floor of commercial structures within the study area. While a mix of uses may be found generally above the retail/service commercial uses along Lafayette Avenue, Orange Avenue exhibits a significant concentration of second floor apartments. The only building along Orange Avenue with a second story that does not have upper floor residential uses is the building located south of and adjacent to the Piermont Branch. Residents are primarily in the study area during the evening hours, as well as weekends.

Retail/service commercial uses are heavily concentrated along Lafayette Avenue, the main commercial spine of the central business district, and primarily on the first floor. Exceptions include a karate instructional school, a billiard showroom, and a hair and nail salon, located on second floors.

Retail/service commercial uses are also found on the first floor of structures along Orange Avenue, primarily concentrated in the Comesky Block. There are no retail/service commercial establishments on the second floor of any establishments along Orange Avenue. Most retail/service commercial establishments operate during daytime hours.

Eating and drinking establishments with extended evening hours are generally concentrated along the lower end of Lafayette Avenue and along Orange Avenue. Eating and drinking establishments further east along the upper end of Lafayette Avenue are primarily limited to coffee and lunch shops, and do not operate late into the evening. These uses are the primary and often the only destination for nighttime patrons to the study area.

Office uses are found on the second floor of structures located along Lafayette Avenue. An office use is also located on the second floor of a building located adjacent and south of the Piermont Branch along Orange Avenue. First floor office uses are found along Washington Avenue, north of its intersection with Lafayette Avenue (Avon Benefits office). Office uses include real estate, engineering services, businesses and attorney establishments. Offices generally maintain daytime hours, and do not have spinoff benefits, since they do not attract a significant number of potential patrons into the downtown district. As a result, their location on second floor levels is appropriate.

The study area has an impressive number of banking establishments, five in total, all located along Lafayette Avenue. With the exception of one bank located in the Lafayette Theater Building, all banks have drive-thru facilities. The number of drive-thrus in the study area has some impact on the pedestrian environment within the central business district, described in later sections of the study. These facilities attract a substantial number of customers into the downtown, during the day. However, the drive-thru facilities, by their very nature, allow customers to conduct their transactions without leaving their car, avoiding interaction with the downtown environment. As a result, the spinoff benefits from bank patrons entering the business district are not as significant as would have been without the drive-thrus. The banking hours generally cater to daytime customers.

Auto-related uses are located primarily along Wayne Avenue, with the exception of an auto-repair establishment located in the vicinity of the Chestnut Street overpass along Orange Avenue. A tire dealer, and several auto repair yards are found along Wayne Avenue. One of the yards associated with an auto-repair establishment also fronts along Hallett Place. These uses are carried out on the first floor of buildings, and in unenclosed yards. These services are provided in the daytime, and the uses generally do not operate into evening hours. They are often unkempt and not as pedestrian friendly, with cars parked on the sidewalk and across the street curb.

Non-retail commercial includes a variety of uses scattered throughout the conservation district study area. These include the Avon warehouse fronting along Washington Avenue, extending west to Suffern Place; metal fabricators along Chestnut Street and Orange Avenue, south of Lafayette Avenue; a plumbing wholesaler located along Suffern Place, in addition to an establishment assembling and storing stage sets; an HVAC supplier and contractor, furniture restoration, catalog sales and storage, and molding manufacturer, along Wayne Avenue. These uses generally have daytime hours of operation, and do not cater to the general public. Their employees, however, may utilize retail/service commercial services within the study area, as well as eating and drinking establishments and banks.

Most public facilities and services are located on the periphery of the study area, and also provide a sense of transition between the village's outlying residential areas and the business district. Village Hall, Sacred Heart Church, and Avon Park are located along Washington Avenue, the Congregation Sons of Israel synagogue is located along Suffern Place, and a vest-pocket park is located at the intersection of Lafayette and Orange Avenues. A U.S. Post Office is located along Chestnut Street north of its intersection with Lafayette Avenue. A firehouse is located along Wayne Avenue, and a bus depot is located along Orange Avenue. A youth counseling center is located along Lafayette Avenue within the Comesky Block. With the exception of the places of worship and Village Hall, patronage for these facilities is during weekday, daytime hours. The religious institutions also experience patronage during evening hours and weekends. Village Hall is used for meetings and programs many weekday evenings and on weekends.

Parking lots are located throughout the downtown district, both municipal owned and/or maintained, as well as private lots for patrons and residents of the study area. Three municipal lots are located within the study area: one is located south of Lafayette Avenue behind the Miranda Building, with access provided to the lot from Lafayette Avenue via an alleyway, East Park Place and Washington Avenue, through the theater parking area; another is located north of Lafayette, with access from Chestnut Street and Suffern Place; and the third is located along Orange Avenue in the vicinity of the rail depot.

Private parking areas are located behind structures, within side yards, as well as on individual lots, along Lafayette Avenue, Washington Avenue, Park Place and Chestnut Street. Municipal off-street parking is least available in the Lafayette/Orange quadrant.

B. ZONING

Properties within the conservation district study area are zoned in accordance within the following five categories:

- **2R-5(P) Two-Family Residence** (one and two family dwellings; public facilities)
- **MR-15 Multi-Family Residence** (low-rise multiple residences at maximum gross density of 15 dwellings per acre)
- **MR-50 Multi-Family Residence** (high-rise multiple residences at maximum gross density of 50 dwellings per acre)
- **CB Central Business**
- **GB General Business**
- **PLI Planned Light Industrial**

Figure 4 illustrates the properties within the study area according to their zoning designations.

Table 1 lists the permitted and special permit uses allowed in each district; **Table 2** describes the bulk regulations for each district.

**Figure No. 4
ZONING**

LEGEND

Residential Districts:

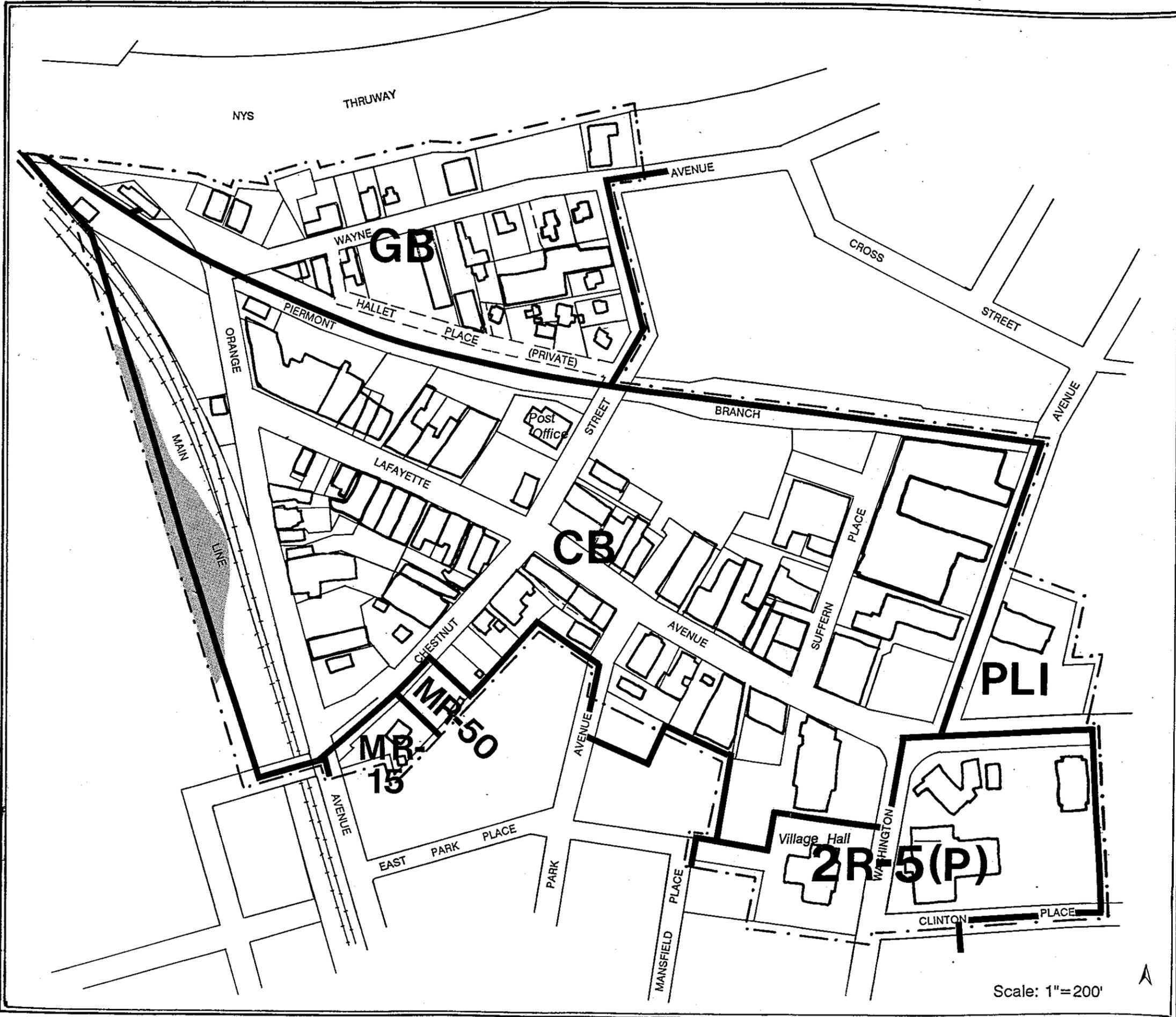
- 2R-5(P)** Two Family Residential
- MR-15** Multi-Family Residential
- MR-50** Multi-Family Residential

Non-Residential Districts:

- CB** Central Business
- GB** General Business
- PLI** Planned Light Industrial

Overlay District:

-  Floodplain District



VILLAGE OF SUFFERN
 * CONSERVATION DISTRICT *
 STUDY

**TABLE 1
PERMITTED AND SPECIAL PERMIT USES WITHIN THE STUDY AREA**

ZONING DISTRICT	PERMITTED USES	SPECIAL PERMIT USES
2R-5(P)	<ul style="list-style-type: none"> ● One-Family Detached dwellings ● Places of worship ● Public emergency and nonemergency facilities ● Not-for-Profit membership associations serving the general public. 	<ul style="list-style-type: none"> ● Public Utility Building Substations; Utility Lines and Poles Serving 25 KW or greater; Standpipes and Water Towers ● Private schools, colleges ● Dormitories accessory to schools ● Private and public elementary or secondary schools ● Nursery schools, Day care centers ● Home occupation
MR-15	<ul style="list-style-type: none"> ● One-family detached dwellings ● Two-family dwellings ● Places of worship ● Low-rise multiple dwellings ● Office use existing at the time of adoption of this amendment to the local law shall be deemed to be a non-conforming use 	<ul style="list-style-type: none"> ● Professional Office of Studio
MR-50	High rise multiple dwellings	<ul style="list-style-type: none"> ● Professional Office or Studio
CB	<ul style="list-style-type: none"> ● Places of Worship ● Professional and Business Offices ● Banks ● Retail Stores, Personal Services ● Restaurants ● Fast food Restaurants in Multi-Tenant Buildings ● Indoor Theaters ● Printing and Lithographing ● Schools of Special Instructions (not permitted on ground floor) ● Public parking facilities ● Plumbing and HVAC Supply ● Funeral Parlor 	<ul style="list-style-type: none"> ● Public Utility Building and Sub-stations; Utility Lines and Poles serving 25 KW or greater; Standpipes and Water Towers ● Private Membership Clubs ● Live Entertainment ● Bus Terminals ● Microwave transmission and receiving antenna and dishes
GB	<ul style="list-style-type: none"> ● Public Emergency Facilities ● Professional and Business Offices ● Banks ● Retail Stores, Personal Services ● Restaurants ● Indoor Theaters ● Schools of Special Instruction ● Franchise Automotive Sales and Service ● Public parking facilities ● Indoor Recreation ● Printing and Lithographing ● Plumbing and HVAC Supply ● Wholesaling, Warehousing Distribution ● Funeral Parlor 	<ul style="list-style-type: none"> ● All special permit uses in the CB District ● Kennels, Veterinary Offices, Animal Hospitals ● Automobile Washing Establishment ● Automotive Repair ● Betting Parlors ● Bus Terminals
PLI	<ul style="list-style-type: none"> ● Professional and Business Offices ● Research and Development Laboratories ● Schools of Special Instruction ● Wholesaling, Warehousing Distribution ● Assembling or Finishing of Products ● Printing and Lithography ● Production, fabrication, processing, converting, altering, assembling, packaging, testing or other handling of products ● Public Emergency Facilities 	<ul style="list-style-type: none"> ● Public Utility Buildings and Sub-stations; Utility Lines and Poles Serving 25 KW or greater; Standpipe and Water Towers ● Excavation of Mineral Resources ● Private Membership Clubs ● Helipads ● Microwave transmission and receiving antenna and dishes

Source: Village of Suffern Zoning Local Law, 1994.

**TABLE 2
BULK REGULATIONS APPLICABLE WITHIN THE STUDY AREA**

District		MIN. LOT AREA SQ. FT.	MIN. LOT WIDTH	MIN. FRONT YARD	MIN. SIDE YARD	MIN. TOTAL SIDE YARDS	MAX. DEVELOPMENT		MAXIMUM STORIES	HEIGHT FEET
							REAR YARD	% OF LOT AREA		
2R-5(P)	One Family Dwelling	5,000	50	20	10	20	20	35	2½	35
	One Family Semi-Attached Dwelling	4,000/unit	40	20	20	20	20	35	2½	35
	Two Family Detached Dwellings	8,000/unit	80	20	15	30	20	35	2½	35
	All Other Permitted Uses	60,000	100	25	25	50	30	30	2½	35
MR-15	One Family Dwelling, Professional Office or Studio	5,000	50	20	10	20	20	35	2½	35
	One Family Semi-Attached Dwellings	4,000/unit	40	20	20	20	20	35	2½	35
	Two Family Detached Dwellings	8,000/unit	80	20	15	30	20	35	2½	35
	Multiple Dwellings (Low Rise)	40,000 Plus Min. Area From Schedule A, p.2	100	25	25	50	40	30	2½	35
	All Other Permitted Uses	60,000	100	25	25	50	30	30	2½	35
MR-50	Multiple Dwellings (High Rise)	40,000 Plus Min. Area From Schedule B, p.2	100	25	25	50	50	20	6	70
	All Other Permitted Uses	20,000	100	25	25	50	30	30	2½	35
CB ¹	All Permitted Uses	None	25	15	None	None	20	70	4	45
GB ²	Business, Professional, Gov't. Office, Restaurants, Indoor Theaters, Public Buildings	None	25	15	None	None	20	70	4	50
	All Other Permitted Uses	None	50	30	15	30	30	60	2½	40
PLI	All Permitted Uses	40,000	100	35	40	80	40	80	3	40

SCHEDULE A: MR-15 DISTRICT ADDITIONAL LOT AREA REQUIREMENTS

<u>APT. SIZE</u> (as determined by the Planning Board)	<u>SQ. FT. OF ADDIT. LOT AREA</u>
1 - 1½ (No Bedroom)	2,200
2 - 2½ (One Bedroom)	2,700
3 - 3½ (One Bedroom)	3,200
4 - 4½ (Two Bedroom)	3,700

No More Than 4½ Rooms Per Dwelling Permitted

SCHEDULE B: MR-50 DISTRICT ADDITIONAL LOT AREA REQUIREMENTS

<u>APT. SIZE</u> (as determined by the Planning Board)	<u>SQ. FT. OF ADDIT. LOT AREA</u>
1 - 1½ (No Bedroom)	300
2 - 2½ (One Bedroom)	450
3 - 3½ (One Bedroom)	750
4 - 4½ (Two Bedroom)	950

No More Than 4½ Rooms Per Dwelling Permitted

1. CB DISTRICT -
 - A. The minimum side yard for properties adjoining residential districts shall be 20 feet. Where a side yard exists, the minimum dimension shall be 15 feet.
 - B. Front yard requirements may be adjusted to an average setback based on dimensions of existing properties adjacent to the property in question.
2. GB DISTRICT -
 - A. Where a side yard exists, the minimum dimension shall be 15 feet.

Source: Village of Suffern Zoning Local Law, 1994.

1. 2R-5(P) Two-Family Residence

The portion of the study area zoned 2R-5(P) includes the Village Hall and the Sacred Heart Church complex. This zoning district was created as part of the recent zoning local law amendments recognizing that Washington Avenue is the civic center of the village.

2. MR-15 Multi-Family Residence

Within the study area, only one property is zoned MR-15, located at the intersection of Chestnut Street and Orange Avenue. It is part of a larger district that extends south along Orange Avenue, and includes several properties fronting on East Park Place. As part of the comprehensive amendments to the zoning local law, this district was established to encourage the construction of multi-family housing units in the vicinity of the train station and to replace units lost through the conversion of apartments in the CB and GB district to nonresidential uses. The existing two-family dwelling is a conforming use within the district.

3. MR-50 Multi-Family Residence

A portion of the property containing the high rise apartment complex on Park Place extends to Chestnut Street. The apartment complex's pool is located on this portion of the property. A wooden slat fence has been erected to screen views into the property. It is topographically separated from the larger portion of the parcel along Park Place.

4. CB Central Business

The Central Business zoning district encompasses the commercial center of the village, and extends from Washington Avenue to the rail property west of Orange Avenue. Its northern limit is the centerline of the Piermont Branch, and its southern boundary encompasses properties fronting on Lafayette Avenue. It south along Chestnut Street to include properties north of the Orange Avenue/Chestnut Street overpass.

The primary nonconforming uses found within the CB district include upper floor residential uses, and Avon's storage and warehousing buildings located between Washington Avenue and Suffern Place.

The Suffern Place vicinity was originally zoned for manufacturing under the 1977 zoning law. The rezoning of this property was addressed in a May 16, 1993 memo to the Suffern Planning Board, confirming the zoning district changes that had been agreed to by the Board, acting as the Rezoning Committee. In general, the committee felt that these structures, in the long-term, should eventually convert to commercial uses because of their location within the commercial center of the community. The Village Board concurred, and rezoned this area to CB in November 1993.

Other nonconforming uses within the CB zoning district include an auto-related use along Orange Avenue at the intersection of Chestnut Street, and two metal fabrication establishments, one along Chestnut Street, and another along Orange Avenue. These uses were nonconforming under the 1977 zoning law and remain nonconforming under the present law.

To encourage the infill of existing properties in the CB zoning district in a manner consistent with the current pattern of development, no minimum lot area or side yard requirements are prescribed (side yards are required adjacent to a residential district). A minimum rear yard is required to provide additional access for fire protection purposes.

There is a minimum front yard requirement of 15 feet for buildings within the CB district, however, the front yard may be reduced to be consistent with the setbacks of adjacent buildings.

5. GB General Business

The General Business zoning district encompasses properties along Wayne Avenue, and north of Hallett Place within the study area. This is consistent with the 1977 zoning local law. Like the CB district, there is no minimum lot requirement within this district, and no side yard requirement for the uses that are the same as those permitted in the CB district. However, other uses, e.g., wholesaling, warehousing, and auto-related uses, require minimum side yards. The existing residential uses within this district are nonconforming. It is the Village's goal for these properties to eventually convert to nonresidential uses. Because of the existing topography in this vicinity, and the lack of available parking areas either on-site or off-site in the vicinity, most of the residences will likely remain. Auto-related uses will probably continue since they represent conforming uses in this district.

If the community desires the existing residences to convert to nonresidential uses, e.g., office, restaurant etc., then the issue of parking may need to be addressed for this portion of the study area.

6. PLI Planned Light Industrial

Within the study area, the PLI district is confined to the office property located at the northeast corner of Washington Avenue and Lafayette Avenue. It is a conforming use within this district and is part of a larger zoning district that includes the Avon office headquarters and manufacturing facility.

C. PHYSICAL ATTRIBUTES OF EXISTING PROPERTIES AND STRUCTURES

An important component of the conservation district study is to characterize existing buildings in the project area with regard to the following attributes:

- architecture
- types of buildings
- building materials
- dates of building construction
- details

The existing building survey is intended to achieve the following:

- determine whether a consistency of building types exists in the study area
- identify building attributes that should be preserved
- determine whether certain features should be incorporated into new development within the study area

Appendix A contains a matrix of building and property features.

As noted previously, the study area consists of a mix of residential and auto-related non-residential uses on the periphery of the study area, and a solid retail/service commercial core along Lafayette Avenue. This land use pattern is reflected generally in the building types and architectural styles within the study area.

Buildings within the study area represent an eclectic architectural mix representative of Suffern's evolution from a small crossroads hamlet to a bustling village center, a period in time spanning roughly from the 1840s to the 1920s, when much of the study area developed. The year 1841 marked the commencement of rail travel along the Erie Railroad, from Piermont to Goshen and on to Dunkirk, south of Buffalo, with a stop in Suffern. In 1848, a second line was established, carrying passengers between Jersey City, with its connections to New York City and Suffern. The development of the railroad spurred village growth. In particular, the turn of the century witnessed the construction of many of the brick commercial buildings lining Lafayette Avenue that still stand today.

By the 1920s, much of the Village's central area was developed. Other changes, infills, and additions to existing structures have occurred since the 1920s, and their architecture is modern in character.

1. Residential Structures

Within the study area, residential structures are characteristic of the Romantic (1820-1880) and Victorian (1860-1900) periods. These structures have gable fronts or cross gable fronts, and several retain architectural detailing suggesting a particular style, e.g., brackets, spindlework, etc. However, the majority of the residential structures in the study area have been altered over time, and the features that would have defined their particular architectural style, e.g., Italianate versus Gothic Revival, have been lost. In addition, these structures may have combined styles; regardless, the lack of ornament today makes it difficult to characterize buildings as to their specific architectural style.

An exception is the Second Empire structure located along Wayne Avenue - it may be identified by its characteristic mansard roof.

2. Residential Conversions

Several structures within the study area represent residential building types altered by the addition of a single story ground level retail storefront. Examples include the building located directly adjacent to the former Suffern Hotel, and the row of buildings along Chestnut Street adjacent to and south of Suffern Succotash. The first floor storefronts have masked any architectural detailing that may have existed prior to their ground level additions. Often, the remaining detailing in the upper portion of the building is not seen from the sidewalk in front of the building, but may be visible from across the street or from a car.

3. Commercial Buildings

A number of the commercial buildings within the downtown environment were constructed in the time period between the 1900s to the 1920s. These handsome buildings are between two and three stories with brick facades, and have retained ornamental details including moldings and cornices. Windows are pronounced because of the use of large stone or brick sills and lintels, often with keystones. Several of the structures also have arched windows framed by patterned brickwork. The block along the south side of Lafayette Avenue between Park Place and Chestnut Street represents one continuous block of commercial row buildings that incorporate these features.

The period after the 1900s saw the construction of buildings influenced by period revivals - the Comesky Block appears to be influenced by this trend.

The Comesky Block appears to mimic Spanish Colonial details, because of its use of tiled roofs and tile accents beneath the brackets supporting the cornice of the building. Other buildings that appear in this style include the Suffern Furniture building, and the Gift Cupboard building. In addition, "tudor" styles also were popularized - the building with timber decorative details adjacent to Hines Playhouse along Lafayette Avenue appears to have been constructed in this fashion.

One story structures within the study area provide varying details - some have been decorated with timber and stucco elements, suggesting a Tudor or Spanish Colonial appearance. Along Lafayette Avenue, many one-story structures have brick parapets that are articulated on top.

"Public" buildings constructed in the early to mid-1900s are grand in scale, and their architecture reflects their important stature in the community. An example of a significant public building is the Bank of New York structure located at the corner of Park Place and Lafayette Avenue. Sacred Heart Church at the corner of Lafayette and Washington Avenues and Village Hall are other examples.

Several modernistic structures are found in the study area, and their appearance can be identified generally by their utilitarian features and lack of ornamentation.

Recent additions to the downtown appear to have attempted to mimic styles prevalent in the business district. The strip mall located on the north side of Lafayette Ave. between Orange Ave. and Chestnut Street includes timber and stucco decoration. The Miranda Building, located on the south side of Lafayette Avenue adjacent to the Lafayette Theater, reflects a modern interpretation of the commercial rowfronts built in the early 1900s.

4. Other Building Types

Auto-related uses are generally located along Wayne and Orange Avenues, and are housed in a variety of building types, ranging from one story garages and sheds to two-story commercial structures, in brick or wood frame.

Large box-like warehouse structures dominate the northwest corner of Lafayette and Washington Avenues north of Avon Park and the Congregation Sons of Israel synagogue.

A former freight station, dating to 1887, is located within Conrail's right-of-way.

Figure 5 identifies Buildings of Interest.

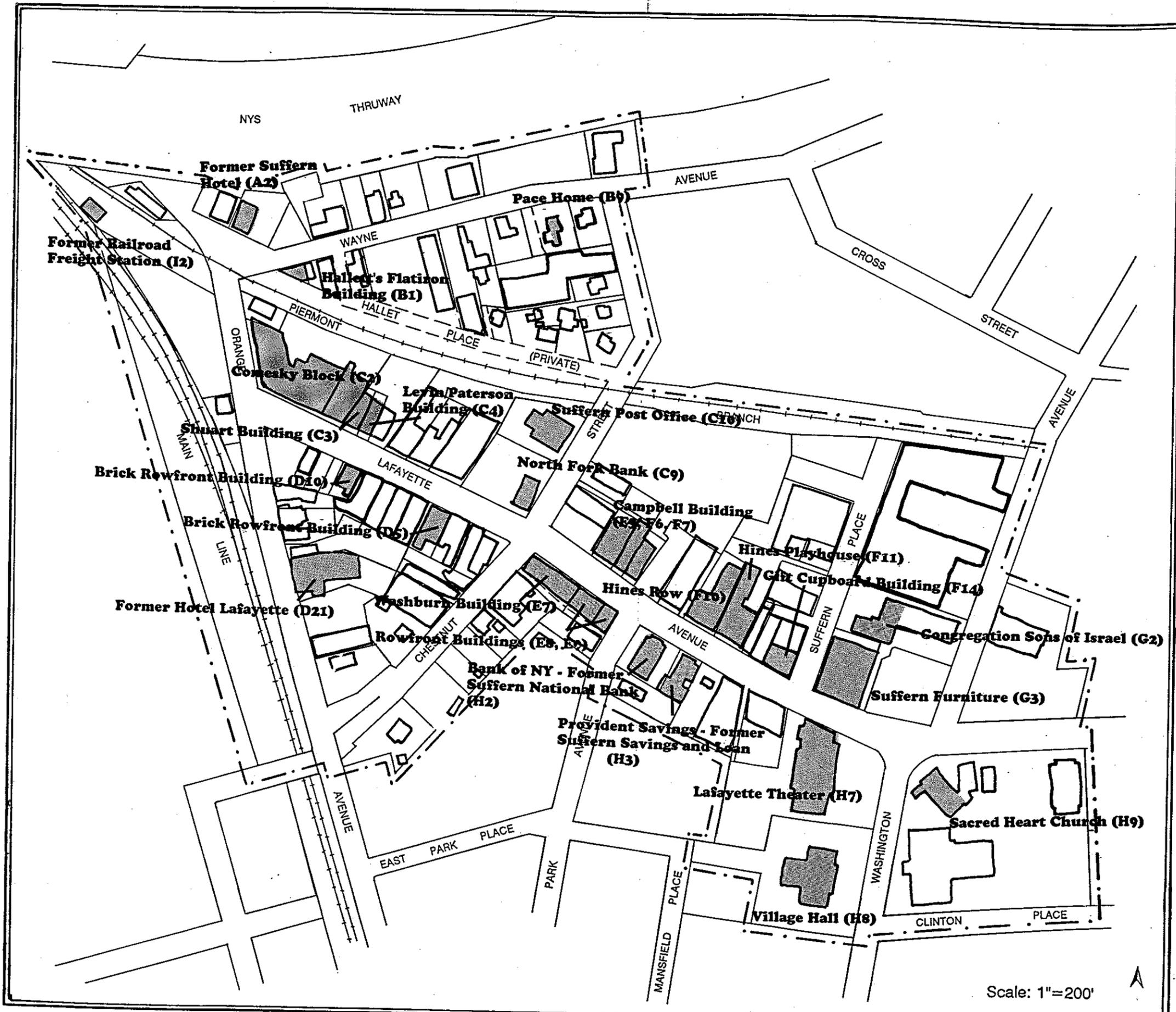


Figure No. 5
BUILDINGS OF INTEREST

LEGEND

- (A1) Property Identification Number
- Building of Interest

Refer to Appendix A for description according to Property Identification number.

VILLAGE OF SUFFERN
 * CONSERVATION DISTRICT *
 STUDY

Scale: 1"=200'

D. STREETScape

1. Signs

A uniformity of sign types can be found on buildings recently retrofitted with signs funded by the Suffern Chamber of Commerce. The objective of this Chamber of Commerce program is to reduce the cluttered appearance of the central business district, and make it a more attractive and appealing place to visit. The Comesky Block provides an example of this uniformity, utilizing similarly sized signs and coordinated colors that are consistent with the colors of the building's facade.

An example of a building displaying multiple signage treatment is the Miranda Building, with its collection of facade and awning signs. This collection can result in a "cluttered appearance."

Rolltop awnings are being replaced systematically, an activity also sponsored by the Chamber of Commerce. New roll-down awnings are attractive and consistent with the building facade's colors to which they are attached.

There are several "icon" signs that remain within the downtown area, even though the business to which a particular sign belonged is no longer in operation. An example of an icon sign is the barber pole attached to the building south of Lafayette Avenue, on the west side of Chestnut Street. Icon signs, when maintained properly, can add visual interest to the village's environment.

Two freestanding signposts without signs are located at the entry to the study area, along Orange Avenue, and associated with the auto-repair establishment. These signposts should be rehabilitated or removed, since they visually detract from the entry into the central business district along this thoroughfare.

Paper window signs proliferate throughout the study area, particularly associated with retail establishments, e.g, delis, liquor stores, and tend to clutter the appearance of the study area.

2. Street Furniture

The study area is characterized by a general lack of street furniture. These features are limited to the garbage receptacles placed along the sidewalks along Lafayette Avenue, Orange Avenue and Chestnut Street. Wayne Avenue, since it does not accommodate a large number of pedestrians, is not furnished with these receptacles. This is also true of the minor roads within the study area, including Park Place and Suffern Place.

A field survey of the downtown found that there are no street benches within the study area, with the exception of the benches located in front of Village Hall. The parks within the study area do not have benches, although Avon Park does have a gazebo which people use for sitting. Visitors have been viewed sitting along the brick planters adjacent to North Fork Bank, the retaining wall located in the Marine Midland Plaza, and the steps to the Bank of New York and Provident Savings.

A community bulletin board is provided adjacent to Washington Avenue in the vicinity of the driveway leading into Village Hall. However, this board is not easily visible, since it is located outside the primary pedestrian circulation routes within the study area, e.g., Lafayette Avenue.

In general, the sidewalks are too narrow to accommodate seating associated with outdoor cafes or eating places.

3. Street Trees and Plantings

Street trees and plantings are an important element of the streetscape. Trees have the following benefits:

- provide shade during the summer
- soften the appearance of impervious surfaces
- provide visual relief within the streetscape
- add a positive visual component to the landscape

Mr. Paul Trader, Cornell Cooperative Extension agent, inspected street trees within the study area. He has provided information on the following:

- the location of the tree
- data regarding each tree, in terms of shape, placement, planting pits, grates, maintenance, design, and tree condition
- appropriate specifications for tree selection and planting within an urban environment

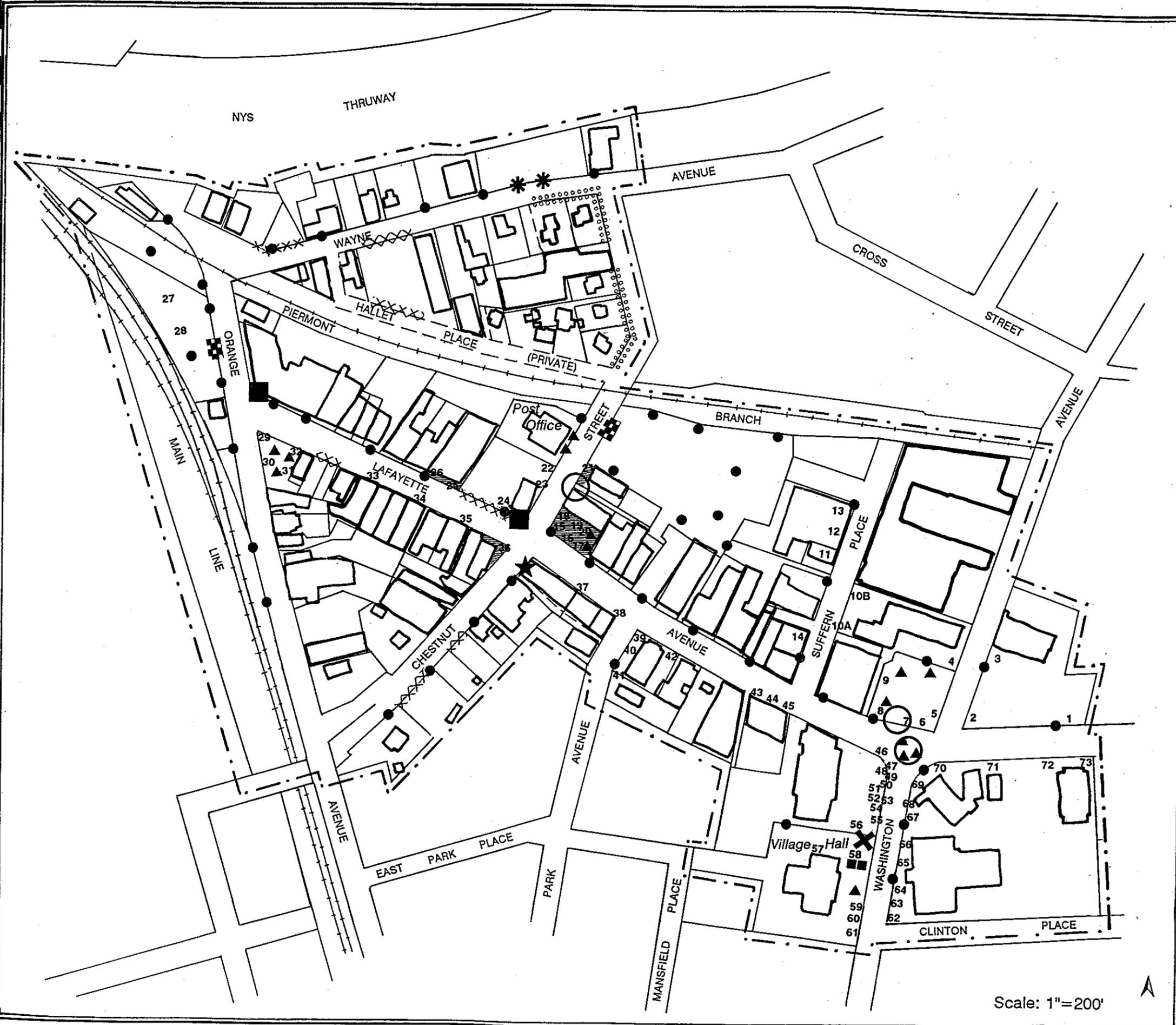
Mr. Trader's report is included as **Appendix B**. The results of his findings are summarized here. **Figure 6** identifies the location of street trees within the conservation district study area.

According to this report, approximately 60% of the study area's street trees are maples. A concern exists regarding the potential that a disease or pest attack on maples could wipe out a large percentage of the tree population in the study area.

Figure No. 6
STREETSCAPE
FEATURES

LEGEND

- 17 **Tree Numbers**
(refer to Appendix B for description)
- **Street Lighting**
- ▲ **Decorative Lamps**
- ▨ **Plazas**
- **Monuments**
- ⊗ **Front Yard Fencing**
- ⋯ **Stone Retaining Walls**
- * **Decorative Stone Pillars**
- ⊞ **Bus Stops**
- **Clock Towers**
- ★ **Mural**
- ⊗ **Community Bulletin Board**
- **Benches**



VILLAGE OF SUFFERN
 * **CONSERVATION DISTRICT** *
STUDY

In terms of shape, many of the trees that have been planted will grow to have large crowns or canopies. This may be a problem where inadequate separation exists between a tree and a multi-story building.

Trees were ranked overall in terms of their condition: excellent, good, fair, poor, dead. Only three of the street trees were ranked excellent. The majority of trees were ranked as being in either good or fair condition. None of the trees were dead; however, a fair number of Norway maples were rated as being in poor condition, and should be removed and replaced.

Tree trunks bear the scars of cars and trucks backing into them. These trees are not placed far enough away from the curb to protect them, nor have any protective structures, e.g., bollards, been provided. Protective measures have similarly not been provided horizontally, e.g., metal grates to protect the roots and trunk. Many of the trees are scorched or stunted due to small planting pits. Trees that are still alive require general maintenance, including pruning of dead limbs, and fertilizer applications.

Some opportunity exists to plant additional street trees within the study area. Several locations, e.g., south side of Lafayette Avenue between Chestnut Street and Park Place, have adequate sidewalk width to accommodate street trees. In addition, overhead utility lines have been relocated to the rear of existing buildings on Lafayette Avenue, therefore, the usual conflicts between utility maintenance and tree growth are not present along this corridor.

Other areas, including Wayne Avenue, do not maintain adequate sidewalk width to provide street trees.

In addition to street trees, several planters are maintained in the downtown district, with flowering plants and ornamental shrubs planted to provide visual relief from the concrete sidewalk. Also, narrow hedgerows and flower boxes have also been planted to soften the visual environment.

The detached residential buildings located within the study area are generally set on lawns, with landscaped front yards providing greenery in the study area. Along Lafayette Avenue, the bank properties, including Provident Savings and the Bank of New York, also maintain landscaped front yards, a visual amenity within the study area. Along Washington Avenue, the public facilities, including the church and Village Hall, are attractively landscaped and well maintained.

Other plantings have been provided to screen large expanses of asphalt associated with parking areas. These include the well-maintained hedgerows that screen views of the parking lot at the northeast quadrant of Lafayette's intersection with Washington Avenue, and the landscaped strip provided around the Lafayette Theater parking area at the southwest quadrant of the same intersection. The landscaped strip, however, is physically stressed from numerous crossings that are made between the hedges to access the lot.

The Village has recently received grant money for the purchase and planting of street trees, however, these plantings have taken place outside the study area, primarily along Washington Avenue to the south.

4. Lighting

Lighting is an important element of the streetscape, intended to meet the needs of both Village residents and pedestrian visitors who come to Suffern at night to frequent restaurants, attend the theater, or generally walk downtown for relaxation, and to provide adequate illumination for vehicular movements, including to and from parking areas. In addition to illuminating streets and sidewalks, lighting must also impart a sense of security within the environment.

Street lighting within the public street rights-of-way is primarily limited to standard highway lighting that is functional but unattractive. This type of lighting is adequate for street safety, but does not enhance the visual quality of the study area and does not generally illuminate sidewalks very well. Most of these "highway-type" lights are mounted on wooden poles, and large metal stanchions. This type of highway lighting is found along all the streets within the study area; their location is shown in **Figure 6**.

Portions of the study area are lit by pedestrian-scaled street lamps. Three decorative street lamps are located at the war monument at the intersection of Lafayette Avenue and Washington Avenues. This type of street lamp is also found at Avon Park, casting light onto the sidewalks, and at the entry to Village Hall. The Suffern post office has similar lamps at its entry. The design of the street lamps in these three locations are Victorian in appearance, and attractive. Further west, the vest pocket park located at the intersection of Lafayette and Orange Avenues is adorned with three Colonial style street lamps that light the walks internal to the park. These lamps are different in style to those identified previously. The Marine Midland Bank plaza also has three decorative lamps - these are modern in style.

A downtown walk was conducted on two occasions during the evening hours, to determine the effect of existing lighting and its impact on perceptions of security.

In general, lighting along the segment of Lafayette Avenue between Washington Avenue and Orange Avenue is adequate. Because the street lighting is located on the north side of the Avenue, potential exists for the south side to experience darker spots along the sidewalk. However, many of the ground level establishments have display windows that are internally illuminated, and the spillover effect significantly enhances overall levels of lighting.

Several storefronts use fluorescent lighting within their display windows, e.g., Levine's Pharmacy and Hire Style, and these lights brighten large areas surrounding the storefronts. Lights that illuminate canopies, e.g., Muggs Pub, also provide significant additional lighting along the sidewalk. Likewise, the Suffern Furniture canopy brightens the east end of Lafayette Avenue, as do the theater marquis and the illuminated M&T bank sign.

Neon signs also illuminate the streetscape, and can provide a pleasing night glow, provided use of the lighting is restrained, and not gaudy. Several of the neon signs are used to indicate that a restaurant is "open" - the neon carrot associated with the New Harvest Diner provides visual interest in the night sky.

East of Lafayette Avenue's intersection with Washington Avenue, street lighting was minimal, and dark spots were noted in front of the Sacred Heart Church, on the south side of the avenue. The lighting also did not sufficiently light the north side of Lafayette Avenue although this is the side of the street where the street lights are located. North of Lafayette Avenue, street lighting is similarly inadequate, however, the many security lights along the Avon buildings generally brighten the sidewalks.

Hallett Place is very dark at night, due to the limited amount of street lighting along this private street. Lighting along Wayne Avenue north of its intersection with Orange Avenue is less than that along Lafayette Avenue. However, additional security lights in the vicinity of the firehouse and those placed on utility poles to cast light upon the storage parking areas of the auto-related uses, increases the amount of lighting along this avenue.

In the vicinity of Orange Avenue and Wayne Avenue, street lighting is adequate, with multiple street lights located in the vicinity of the intersection. Some spillover effect also results from the lighting contained within Municipal Parking Lot A. Traveling south along Orange Avenue, street lights illuminate the west side of the roadway, and some dark spots are experienced along the east side of the street, adjacent to the shopfronts at the Comesky Block, until one approaches the corner storefront at Lafayette and Orange Avenues, where external sign lighting brightens this location.

South of Lafayette Avenue, the same problem exists, however, it is mitigated to some degree by the lights of the two restaurant/pubs, brightening segments of the sidewalk.

Along Chestnut Street south of its intersection with Lafayette, lighting is provided on the east side of the street. However, dark spots are exhibited in the vicinity of the vacant lots. On the west side of the street, lighting is generally inadequate, although the convenience store and adjacent "backdoor" to New Harvest diner ameliorates this problem to some degree.

North of Lafayette Avenue, street lighting is provided on the west side of the street, but then switches to the east side of the street north of the rail crossing. Lower level lighting is exhibited, however, since the majority of the properties along this portion of the street are residential, the lighting level is generally adequate. Lighting along Park Place similarly respects the residential character of the area.

Suffern Place would not be well illuminated if not for the security lights located along the Avon Building along this street.

Municipal Lots A and B are adequately lit. However, Municipal Lot L, in the vicinity of Village Hall, would benefit from additional illumination; any illumination must be sensitive to residential properties adjacent to this parking lot. Two pedestrian paths provide access from rear parking areas to Lafayette Avenue; they are illustrated in **Figure 7**. These pedestrian paths are not well lit.

It should be noted that many of the bulbs in the "highway" lights have burned out, resulting in dark spots along the sidewalks. Maintenance of the lighting equipment is important to retain the feeling of security for downtown visitors.

5. Plazas

The Marine Midland Bank utilizes brick to decorate the plaza in front of the bank. In the vicinity of the Priya Indian restaurant, on the north side of Lafayette Avenue between Orange Avenue and Chestnut Street, bricks also line the internal sidewalks leading to the storefronts associated with this strip plaza. Slate has been provided in front of the Balsamo, Byrne & Cipriani law office along Lafayette Avenue, across from the Post Office, as well as the plaza in front of the Sacred Heart Rectory, along Lafayette Avenue. A large concrete and blacktop plaza is located in front of the drycleaning establishment at the corner of Lafayette Avenue and Chestnut Street.

Figure No. 7
**CIRCULATION
 and PARKING**

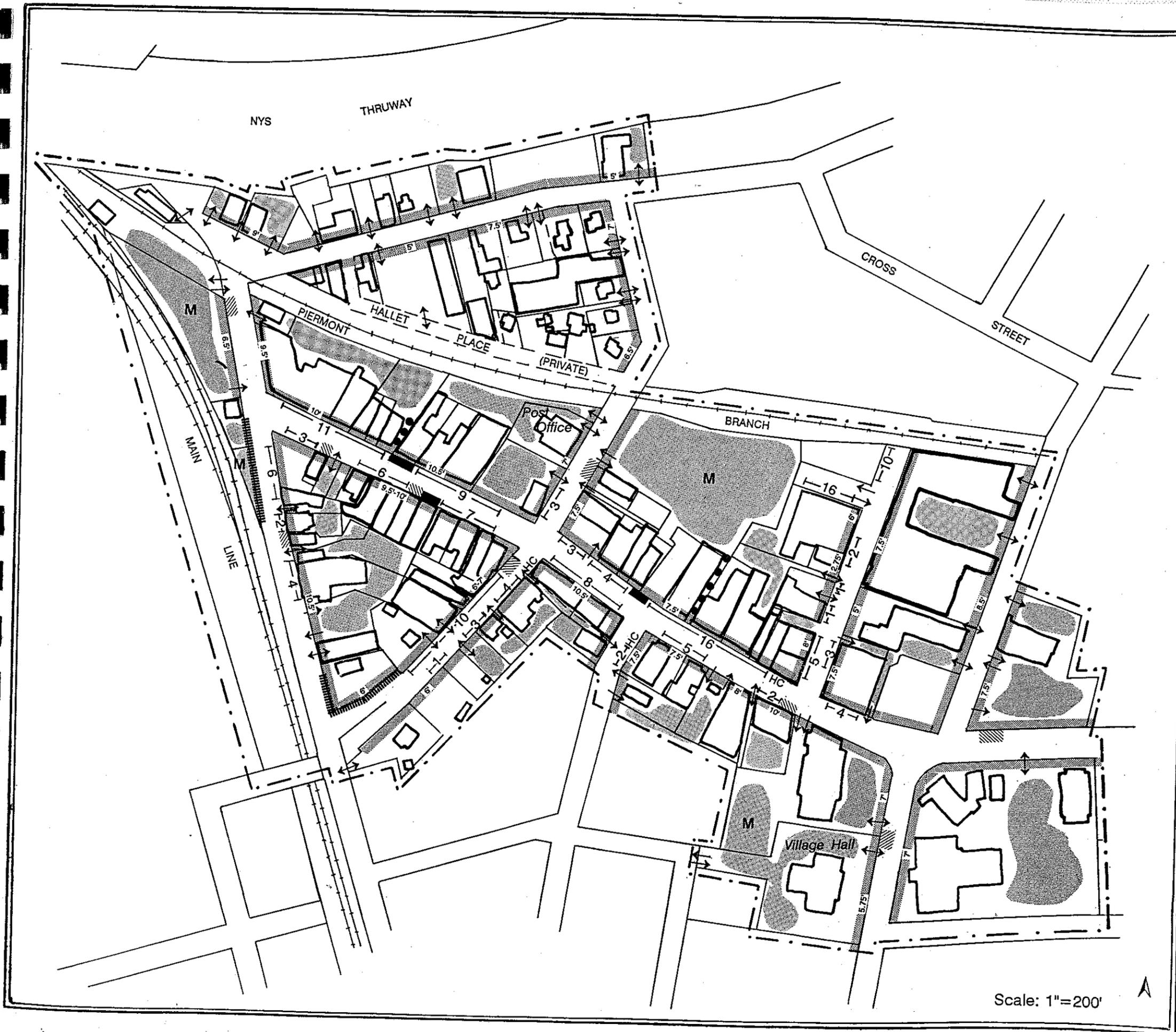
LEGEND

-  **Sidewalks/Sidewalk Widths**
-  **Other Pedestrian Paths**
-  **Off-street Parking**
M = municipal
-  **On-Street Parking Spaces**
-  **On-Street Handicapped Parking**
-  **On-Street Loading (no parking)**
-  **Hydrant locations (no parking)**
-  **Access to parking (arrow indicates direction)**
-  **Uncontrolled Access (no curb)**

NOTES:

1. All roads are two-lane, bi-directional.
2. Parking accessory to auto-related uses not illustrated.

VILLAGE OF SUFFERN
 * CONSERVATION DISTRICT *
STUDY



6. Other Visual Features

There are several important visual features within the study area that enhance the overall aesthetic character of this portion of the Village. Features associated with buildings are described in **Section II.C**. Two clock towers, one located on the second floor of the Comesky Building at the intersection of Lafayette and Orange Avenues, and a ground level clock at North Fork Bank, provide visual interest. A mural that has been recently painted on the blank wall of Suffern Succotash, facing onto Chestnut Street, also provides visual interest, and has received many favorable comments.

Several buildings serve to terminate vistas along several of the roadways that traverse the study area. These include the former Suffern Hotel, terminating the vista northbound along Orange Avenue, and the war monument and Sacred Heart Church, terminating vistas at the intersection of Lafayette and Washington Avenues. These handsome buildings are an important asset to creating a sense of enclosure within the study area, and a sense of entering and exiting the village's commercial center.

Some time ago, the Village removed utility poles along Lafayette Avenue, and placed them behind the buildings within parking areas and rear yards. This eliminated the cluttered appearance that can result from utility poles and their electrical lines, and is a significant positive benefit.

E. CIRCULATION AND PARKING

Suffern's central business district evolved before the introduction of the automobile as a means of transportation. Walking was the first and foremost mode of transit within the central business district, and from surrounding residential neighborhoods, and is still an important component of the study area's circulation system. Today, many visitors and employees to the study area arrive and depart by car, therefore, convenient and efficient vehicular access and parking within the study area is very important. This is particularly true as Suffern's businesses must compete with the strip shopping centers and their usual ease of parking and circulation.

A survey of circulation and parking needs would not be complete without a discussion of the mass transit facilities within the village, including rail and bus transportation.

1. Pedestrian Circulation

The village is responsible for maintaining an extensive pedestrian circulation system within the study area. The existing quality of the pedestrian environment is characterized in this section. **Figure 7** illustrates the characteristics of the pedestrian circulation system.

Sidewalks in the study area vary in terms of their width, quality, and to a lesser degree, their materials. Sidewalks throughout most of the community are concrete. However, decorative pavers have been provided in front of Marcello's, an Italian restaurant at the west end of Lafayette Avenue. Along Wayne Avenue, on the north side of the street east of the former Suffern Hotel, a slate sidewalk surfaces. Also along Wayne Avenue, there are a few locations where the concrete walk is gone, and the sidewalk is blacktop. In a couple of spots, a dirt path is all that remains.

Sidewalk conditions range from poor to good. At the poor end of the scale, sidewalks along Wayne Avenue are in a substantial state of disrepair. Along the north side of the avenue, sidewalk widths range between three (3) to five (5) feet in width, and in several locations, the sidewalk is absent completely. In several locations, utility poles are an obstruction, particularly where the sidewalk is very narrow.

In several locations, blacktop has been used to patch sidewalks, and in one location, slate is used. "Planting strips" are overgrown with weeds and not maintained.

Several of the deficiencies witnessed in the field can be attributed to the auto-related businesses found along the avenue and the impacts they generate. In front of these establishments, curbs are sunk and vehicles are parked and stored along the sidewalk. This not only provides obstructions to the pedestrian, but the sidewalks undergo significant wear and tear from vehicles riding up the sidewalk.

The only other location where vehicles were witnessed parking on the sidewalk was at an existing auto repair establishment located along Orange Avenue in the vicinity of the Chestnut Street overpass.

In addition to these obstructions, other factors contribute to this pedestrian hostile environment, including a lack of buffering between vehicles traveling along Wayne Avenue and pedestrians using these sidewalks.

In the vicinity of Wayne Avenue's intersection with Orange Avenue, the pedestrian crossings along Orange Avenue and pedestrian ways in the vicinity of the rail crossing are also deficient. The unsafe conditions related to unmarked pedestrian crossings is compounded by the confusing vehicular turning movements at the approaches to this intersection.

The pedestrian system elsewhere within the study area is better maintained. Sidewalks range between 2.75' along Suffern Place in front of Corporate Images to approximately ten feet in front of the Comesky Block. Overall, the sidewalks are generally free of obstructions. However, in some locations, e.g., Chestnut Street south of Lafayette Avenue, fire hydrants and street poles are almost centered in the sidewalk, reducing overall sidewalk width.

Sidewalk widths become narrower traveling from this main commercial street into adjoining residential neighborhoods. This is appropriate given the lower volume of pedestrians utilizing sidewalks within the residential areas. Along Lafayette Avenue, the narrowest sidewalk widths are found in the vicinity of the insurance agency and liquor store, west of the avenue's intersection with Suffern Place. Here, widths are approximately 7.5 feet.

The sidewalks are used strictly for circulation purposes; merchandise display and sign boards were not viewed along the sidewalk. The sidewalks are generally too narrow to accommodate outdoor cafes.

Multiple curb cuts were identified in the study area, and some pedestrian conflict occurs between automobiles accessing parking lots and crossing sidewalks in the study area. Examples include the drive-thrus that access bank windows, and alleys leading to rear yard parking areas.

Because many of the buildings front along street rights-of-way, cars exiting rear parking areas must nose into the sidewalk to view traffic along the streets to make their turning movement, creating a conflict between the vehicle and pedestrians.

Multiple curb cuts also have the effect of serving as turnaround locations for vehicles, creating not only a pedestrian hazard, but a vehicular hazard as well. It is possible that these turnaround movements are conducted by individuals trying to find an empty curbside parking space in the direction opposite to which they were travelling.

The sidewalks within the study area are primarily concrete. However, textured pavement has been introduced in front of an Italian restaurant along lower Lafayette Avenue. This improvement was made by the owner of the establishment.

Pedestrian crossings at most of the intersections are misaligned - painted crossings do not generally align with the dropcurb.

Two pedestrian alleys not located within a street right of way provide access from rear parking lots to Lafayette Avenue. One alley is located midblock along Lafayette Avenue between Orange Avenue and Chestnut Street; the other pedestrian alley is located alongside the east side of the Suffern Plaza Building and provides access to Municipal Parking Lot B. These walkways are not well-lit after dark.

Based on a study of downtowns in other communities (refer to Section III), street lamps, street trees, and metered parking can be provided comfortably along sidewalks 10 feet in width. In Park Ridge, these streetscape elements were provided along sidewalks with widths less than 10 feet, where less pedestrian traffic occurs.

2. Vehicular Circulation

The following public streets traverse the study area:

- Washington Avenue
- Suffern Place
- Park Place
- Chestnut Street
- Orange Avenue
- Wayne Avenue
- Lafayette Avenue

The conservation district study area is situated at the crossroads of several high volume thoroughfares, providing intercounty and interstate connections. New York State Highway Route 59 (Route 59) enters the study area from the northwestern reaches of the Town of Ramapo along Orange Avenue. Route 59 continues in an easterly course along Lafayette Avenue to serve eastern Rockland County.

U.S. Route 202 is a state-maintained road that enters the study area along Orange Avenue. It then travels north along Wayne Avenue. Washington Avenue, representing generally the easterly boundary of the study area, provides a shortcut between Wayne Avenue and Orange Avenue, which near this location turns into Franklin Turnpike over the state line in New Jersey. Washington Avenue provides access from northerly residential neighborhoods to Lafayette Avenue and the central business district.

Wayne Avenue, Orange Avenue, and Lafayette Avenue carry significant volumes of intracounty and interstate traffic, and also experience heavy truck traffic. Wayne Avenue is a very narrow road, constrained by existing buildings that front directly along the roadway. In addition, large stone retaining walls holding up the front yards of residential properties along Wayne Avenue also constrain widening of this roadway.

The remaining roads within the study area provide local access between different neighborhoods in the village.

Chestnut Street is a village street linking Wayne Avenue with Orange Avenue further west of Washington Avenue. Chestnut Street provides one of two important links between neighborhoods located west of the rail right-of-way and the central business district. With the exception of this road, the only other connection to the central business district is via local streets in neighboring Mahwah, onto Route 202 which becomes Orange Avenue within the Village boundaries.

Park Place is a village street providing access from Lafayette Avenue to adjacent southerly residential and mixed residential/office neighborhoods. Suffern Place is a dead-end stub located north of Lafayette Avenue, between Chestnut Street and Washington Avenue.

Hallett Place, a private road, is also located within the study area. This two-lane, bi-directional road is not maintained, is used for dumping, and negatively impacts the overall visual character of the study area.

All roads within the study area are two-lane, bidirectional thoroughfares. Signalized intersections are located along Lafayette Avenue at its intersections with Orange Avenue and Washington Avenue. At Lafayette Avenue's intersection with Washington Avenue, the war monument channels traffic at this intersection.

The New York State Department of Transportation intends to realign and signalize the intersection of Orange and Wayne Avenues in front of the former Suffern Hotel. The project (according to preliminary plans) will involve the installation of new sidewalks (extending into Hillburn) along Wayne Avenue, the introduction of separate turning lanes at all approaches to the intersection and new curbs in the vicinity of the intersection, and creation of a landscaped plaza in front of the former Suffern Hotel and adjoining building (refer to **Appendix C**).

3. Curbing and Curb Cuts

Most curbing in the study area is concrete. Along several segments of Wayne Avenue, granite curbing can be found, however, it is sunken in some places. In the vicinity of the heavy commercial uses, much of the curb is gone or sunk, allowing vehicles unlimited access to the front yards of these properties. Vehicles have been observed parking along the sidewalk. This is also true of the auto-related use located along Orange Avenue.

Granite curbing is also found along the south side of Lafayette Avenue between Chestnut Street and Park Place. This provides visual appeal and adds historic ambience to the study area. Along many of the study area's intersections, curbing has been ridden over, and is worn and breaking. Curb drops for pedestrian crossings do not match.

As described previously, a general conflict exists between pedestrians on the sidewalks, and vehicles accessing private parking lots located to the rear of many of the buildings within the study area. **Figure 7** illustrates the numerous access points found within the study area. This also has the effect of reducing on-street parking locations within the downtown area.

4. Parking

Parking is provided on- and off-street, within private and public parking facilities. The Village maintains three municipal lots within the study area - Municipal Lots A, B and L (refer to **Figure 7**). Lot A, located adjacent to the rail right-of-way along Orange Avenue, contains a total of 93 spaces. These include 65 permit spaces, 24 three-hour parking spaces, 3 taxi stands, and 1 handicapped space.

Municipal Lot B is located between Chestnut Street and Suffern Place, and north of Lafayette Avenue. This large municipal lot, recently resurfaced, contains a total of 195 parking spaces. Of this total, 86 spaces are reserved for permit parking, 92 are reserved for three-hour parking, 16 spaces are reserved for Chamber of Commerce members, and one space is provided for handicapped parking. On the two occasions that the study area was visited at night, cars were not parked within Municipal Lot B.

Municipal Lot L, adjacent to Village Hall, contains a total of 59 parking spaces. Twenty-one spaces are for permit parking only, 23 spaces are provided for three-hour parking, and 13 spaces are reserved for Village business. In addition, two spaces are reserved for municipal officials.

The location of on-street parking is illustrated in **Figure 7**. In total, there are 151 marked on-street parking spaces within the study area. Of this total, three are reserved for the handicapped. On-street parking is limited to two hours. It should be noted that while there are no marked spaces along Chestnut Street north of the rail crossing, a sign was observed on the east side of the street indicating that two hour parking is permitted.

Of the approximately 500 public parking spaces located in the study area, 58% are available to downtown visitors in two-hour and three-hour stalls. In addition, five handicapped spaces are located within the study area.

There may be a deficiency of parking spaces along the western perimeter of the study area. Several times, during afternoon and evening hours, cars have been observed parking along the west side of Orange Avenue, within the travelled right-of-way and straddling a drainage ditch.

During evening hours, many cars were surveyed parking in loading areas and striped areas, even with a sufficient number of regular parking spaces available.

Private off-street parking is generally located within the rear yards of the commercial establishments fronting along Lafayette Avenue. Private driveways provide access to these rear parking areas. However, several parking areas are visible along street frontages, including the parking area adjacent to DaNina's in the western end of Lafayette Avenue, the North Fork parking lot, and the Lafayette Theater parking area. Along Chestnut Street, a parking area for an industrial establishment is located on the east side of the street, south of Lafayette Avenue. Along Orange Avenue, Mountain Valley Brew Pub's parking area fronts on the street. Along Wayne Avenue, parking for the former Suffern Hotel also fronts along the avenue.

In addition, the storage of automobiles for auto-related uses is generally visible from the storage lots that front along Wayne and Orange Avenues.

Several of the private parking areas downtown have cyclone fencing that detracts from the visual environment. Along Chestnut Street, one fence has barbed wire.

F. SECURITY

Security is generally not an issue during daylight hours. However, during evening hours, an individual's perception of security may be attributable to a number of factors, including but not limited to:

- adequate lighting
- amount of street activity, i.e., traffic, business hours of operation, other pedestrians
- visibility around corners, driveways
- general maintenance of the streetscape
- local police presence
- crime

The observations related to security are based on two nighttime surveys, intermittent observations, and discussions with village citizens. Mr. Craig Long, a local law enforcement officer, was interviewed to ascertain qualitative information on crime within the downtown area.

Generally, the Lafayette Avenue corridor presents a safe walking environment. In addition, the adjoining parks are well-lit, and individuals have been viewed strolling through these locations. A constant, uniformed police presence was noted, with patrols making the rounds within the study area, and foot patrol also witnessed during the early evening hours.

Generally, there appears to be no perceived concern related to security in the vicinity of the Washington/Lafayette Avenue intersection and southward along Washington Avenue. This perception probably relates to the uses located in this vicinity, i.e., Village Hall, with its police department, and the church facility. Some level of discomfort exists when traveling along Lafayette Avenue further east of the intersection, primarily due to the lack of lighting encountered on this segment of the avenue.

Washington Avenue north of the Lafayette Avenue intersection, particularly north of the synagogue, is quiet during evening hours due to the nature of the facilities in this area, i.e., Avon's storage and warehouse structures. Avon's facilities are well-lit at night, therefore, the streetscape feels secure although there is almost no pedestrian activity during evening hours.

Suffern Place provides some feeling of insecurity, due to the lack of street lighting, street activity, and the nature of the uses on this street. In addition, the overgrown rail right-of-way at the end of the block, and the vacant industrial property north of the tracks, are forbidding.

Park Place, while not well-illuminated, does not provide any concern for security, as the building setbacks are generally large, and the high-rise apartments provides some activity in the neighborhood because of the volume of residents entering and exiting the building.

Chestnut Street generally feels secure. There is some street activity associated with the pub and the deli located on the southern segment of the street, and the residential uses on the north end of the street also provide some level of comfort. The exception to this is the vacant property located on the east side of Chestnut Street and just north of and adjacent to the Piermont Branch. This densely wooded unlit area provides some safety concern.

Hallett Place is a desolate private road forbidding at night due to the lack of activity, lighting, the overall poor condition of the road, and the garbage-ridden rail right-of-way.

Orange Avenue generally feels secure at night because of the amount of street activity that occurs along this segment of the road, including pass-by traffic, individuals visiting the pubs and restaurants, and activity at the bus shelter. Lighting could be enhanced, since it is primarily provided by the shopfronts along this street.

Wayne Avenue, east of its intersection with Orange Avenue, is a forbidding street at night, due to the lack of maintenance of pedestrian walkways, the small sidewalk setbacks from the street, and the lack of street activity other than passing traffic in this vicinity.

There have been some fights in local pubs, and drug abuse is constantly monitored, but there are few incidents of violent crime within the study area.

A perception problem may exist during the summer months, when apartment dwellers and some visitors "hangout" outdoors during the warm weather months. This perception may continue as long as the residential uses remain in the study area.

III. EXAMPLES OF NEARBY VILLAGE STREETSCAPES

A visit was made to the following communities with a distinct "village" character, i.e., a commercial business district with surrounding residential neighborhoods, their growth stemming from their location along a rail line:

- Ridgewood
- Westwood
- Park Ridge
- Hillsdale
- Pearl River
- Allendale

The field survey generally included a review of the following elements:

- Sidewalks - Width and texture, curbing, treatment of street corners
- Streetscape Amenities - Lighting, Street Trees, Street Furniture
- Parking - On-street and off-street, parking signage
- Signage
- Buildings: Architecture, Facades, height, setbacks, treatment of rear facades
- Railroad stations, railroad parking
- Land use

The descriptions provided below are for comparative purposes, and to stimulate ideas on the potential strengths and weaknesses of the study area's environment. A description and analysis of the existing environment of similar communities may help to identify potential strategies for addressing issues that exist in Suffern.

A. RIDGEWOOD, NEW JERSEY

The "main street" in the central business district, Ridgewood Ave., has a continuous seam of stores, with very few driveway interruptions. This gives the sense of a substantial downtown, and a continuous pedestrian system. Uses that require drives, e.g, bank drive-thrus, and uses that were auto-related or require expanses of pavement as part of the operation, are located on a parallel street behind Ridgewood Avenue. As a result, Ridgewood Avenue has the appearance of being very pedestrian friendly. Conflicts are rare between vehicles and pedestrians.

Along Ridgewood Avenue, the majority of buildings directly abut the sidewalk. Signage is limited primarily to facade signs located above first story doorways or display windows. We noted only one exception where a sign is hung on the second floor of a building to identify a second story use. A single building observed with a substantial front yard and containing several retail uses had a freestanding sign, and facade sign to identify these uses.

Perhaps the most noticeable feature related to signage is the absence of paper and advertising window signs. The absence of paper window signs eliminates a cluttered appearance to the central business district. Awnings are in good condition and generally constructed of canvas. The awnings did not appear to be internally illuminated. Several facade signs, however, are illuminated. Neon signs are absent within the downtown business district.

The rear facades are not renovated, and no special design attention is given to those that faced out onto public parking areas. However, probably because of parking demand, the rear yards of most structures are paved and utilized for employee parking. While there is no special treatment of the rear facades, the rear areas of buildings appear to be well-maintained, free of refuse and weeds.

The mix of building types found in Ridgewood are comparable to Suffern's, and reflect an eclectic mix of styles, indicative of period architecture that was popular at the time the structure was constructed.

Retail uses dominate the ground floor of buildings along Ridgewood Ave. Also, several realtors are located along the ground floor. Most offices are located on the upper stories of the buildings. In addition, several multi-story buildings contain residential uses on their upper floors. Based on an earlier conversation with a Building Inspector, the Village regulates apartments within the downtown district by requiring each unit to have a minimum size of 1,200 square feet. As mentioned previously, uses requiring paved areas for auto storage are found along a street parallel to the main street. Here, however, a landscape screen is provided between the sidewalk and the parking areas.

Ridgewood has metered parking downtown. Parallel and angled on-street parking is provided. The layout of parallel parking is similar to that in Suffern. Angled on-street parking is found near the train station, where street width permits. Off-street parking is found to the rear of structures. Off-street parking is a mixture of private parking located within the rear yards of structures, and public parking areas. Signs directing the public to off-street parking are small and not very visible. A landscaped buffer is provided between the parking areas and sidewalks. In several instances, brick walls are utilized to screen views into internal parking areas.

Sidewalks are concrete, but have been cast in smaller blocks to provide some texture. A brick border is utilized as a decorative element between the concrete sidewalk and the curb. Street trees, street lamps, meters and fire hydrants are located near the curb within the brick paved areas.

There appears to be a practical advantage to utilizing pavers - in the event that underground utilities require servicing, there is less disruption, heavy paving equipment is not necessary, and large patches of asphalt are not evident.

The railroad station is very active, and set in park-like surrounds. Grassed and landscaped areas surround the rail parking areas.

The downtown business district is generally devoid of street furniture, except for garbage receptacles. Benches are located in the park and by the railroad station.

Drop curbs are provided at street crossings. Sidewalks and curbing are well maintained.

We noted a new infill building on the side street parallel to Ridgewood Avenue. Parking is provided on the ground level. The buildings's materials and facade mimic the colonial structures in the vicinity. A simple elegant facade sign is provided.

B. WESTWOOD, NEW JERSEY

Westwood's downtown environment is similar to Suffern's, in terms of the mix of uses, building types and signage.

Signage for public parking was not observed.

No special design treatment was extended to rear facades.

On-street parking is provided in a tandem arrangement. Two parking spaces are located between a small "no-parking" area, providing easy access into and out of the parallel parking spaces. There appears to be no relationship between the location of the no-parking areas and street tree or other streetscape amenities, i.e., the no-parking areas could have provided some measure of protection of these elements. Because these no-parking areas are small, they did not appear to be utilized for loading.

Public off-street parking areas are provided to the rear of the "main street." A landscape buffer is provided between the parking area and sidewalk. Also, a treed median is provided internal to the parking area.

Street lamps and street trees are provided at the street's edge, and generally sunk into concrete sidewalks. Street intersections are decorative, and brick pavers sunk between concrete margins are utilized to emphasize pedestrian crossings. The street corner is also delineated by brick pavers.

The community appears to be installing decorative pavers between concrete sidewalks and the curb. Street trees, street lamps, and fire hydrants are located in this margin.

A sign in the vicinity of metered parking indicates that the proceeds are to be utilized for beautification of the downtown.

A former retail warehouse has been renovated into several retail stores. The building is surrounded by parallel, metered parking. Decorative pavers are installed at curbs edge, and meters, street lamps, street trees, and fire hydrants located in this margin. A small plaza area is being created, with decorative pavers and landscaping breaking up the concrete expanse. The warehouse property now mimics a village block.

Street furniture, other than garbage receptacles, was not observed.

C. HILLSDALE, NEW JERSEY

Hillsdale's central business district contains several notable features.

First, its railroad station is set in a park-like atmosphere, and is landscaped.

A community bulletin board is provided in a community park located adjacent to the central business district. Street furniture, i.e., benches, are provided in the park. Except for garbage receptacles, street furniture is not observed adjacent to the commercial establishments. However, one establishment did set out a "Victorian" bench. This may allow pedestrians to sit down during the day, but the bench can be pulled in at night.

Street lamps and street trees are provided at curb's edge. The street lamps are set in a half-moon of decorative pavers. Sidewalks are concrete, and approximately 11 feet in width. Street trees are protected by 4'x4' grates.

On-street parallel parking is unmetered, but limited to hourly parking. Parking is also provided in a tandem arrangement. At several street intersections, where parking is not permitted, the sidewalk is extended out to the width of the on-street parking. This area is utilized for street trees and street lamps set in decorative pavers. This area reduces the street width at pedestrian crossings, increasing pedestrian safety.

Off-street public parking is located to the rear of the main street. A small sign guides visitors to the public parking area.

Rear facades are not treated in any particular manner.

D. PARK RIDGE, NEW JERSEY

In Park Ridge, street lamps, street trees and fire hydrants have been set into a brick paver border. Sidewalks are concrete. The rail station is set in a "park-like" landscaped setting.

One notable feature is the new Independence Bank within the central business district. The building's design mimics existing buildings constructed in the early 20th century, with articulated fenestration (instead of a continuous wall of glass along each story of the building), lintels with keystones, and a medallion and palladian window accentuate the building's entrances. Changes in the brick pattern differentiate the first and second stories of the building. The first floor facade has been stepped back from the second story, and the building has been broken up vertically so that the structure does not appear box-like.

A mansard roofline articulates an otherwise flat roof. The contrast between the Independence Bank and the adjacent building is noteworthy.

While Park Ridge has the same standard highway light fixtures as Suffern, it augments these fixtures with pedestrian-scaled street lamps. The street trees, street lamps, and decorative paving all help to reduce the visual impact of utility poles and overhead wires on the streetscape.

E. PEARL RIVER, NEW YORK

Pearl River is notable for its recent installation of street trees. The tree grates are approximately 5.5' x 5.5'. Larger tree grates are utilized, since the sidewalk area measures approximately 13 feet in width. The grates are accentuated by brick pavers. Sidewalks are concrete. Off-street parking is located behind several stores and side streets.

F. ALLENDALE, NEW JERSEY

Allendale recently underwent significant improvements to its central business district. The full width of the sidewalk is decorated with brick pavers. Pedestrian crossings are demarcated with brick pavers. Street lamps and street trees with grates have been added to the streetscape. Curbs are in belgian block.

The most noteworthy visual feature is the mural found on the side wall of a structure, celebrating the community's centennial.

Allendale has an uncluttered appearance similar to Ridgewood, probably resulting from the limited amount of signage found within the business district. Front yards are landscaped, and simple but elegant freestanding signs are utilized.

IV. RECOMMENDATIONS

A. LAND USE

- Support the conversion of the residential and warehousing-related uses to commercial uses.
- Encourage the conversion of the residential uses along Wayne Avenue and Hallett Place into office-related uses.

B. ZONING

- Do not allow parking within the front yard of any property that fronts on Lafayette Avenue.
- When considering minimum front yard requirements for buildings within the CB district, encourage a setback that will provide a distance of 12 feet (2-foot decorative border and 10-foot paved sidewalk) in width from the front facade to the curb. The front yard should be blended into the existing sidewalk.

C. PHYSICAL ATTRIBUTES OF EXISTING PROPERTIES AND STRUCTURES

- Preserve historic structures within the Conservation District; preserve architectural features and details that provide visual interest and lend the Village its sense of place.
- Encourage the adaptive reuse of physically sound residential structures into commercial and office-related uses. Retain existing decorative elements, e.g., slate roof.
- Encourage the retention and reconstruction of existing stone retaining walls within the study area, associated with existing residential structures.
- Encourage the design of new construction consistent with the architectural qualities of Suffern's turn of the century commercial buildings, e.g., Stuart Building.
- Do not permit blank, ground level facades within the study area.
- Encourage restoration of the old railroad freight station, perhaps as the Village Museum or a railroad museum.

D. STREETScape

1. Signs

- Do not permit paper window signs.
- Encourage the conversion of "modern" awnings to facade signs.
- Preserve the Lafayette Theater Marquee.
- Preserve and encourage icon signs that provide visual interest downtown.
- Remove or rehabilitate two freestanding signs associated with the auto-repair establishment located along Orange Avenue.

2. Street Furniture

- Provide uniform standard for waste receptacles and planters.

3. Street Trees and Plantings

- Provide additional street trees in accordance with a streetscape plan, and where sidewalk width permits.
- Maintain existing street trees within the study area, and rehabilitate tree pits.
- Encourage the use of window planter boxes where space permits.

4. Lighting

- Provide pedestrian scaled lighting within the Village in accordance with a streetscape plan.

5. Plazas

- Rehabilitate "plaza" at the southwest quadrant of Chestnut Street and Lafayette Avenue.

6. Other Visual Features

- Protect Comesky Clock Tower and North Fork Bank Clock Tower.

- Protect the War Memorial and continue its use for ceremonial purposes (i.e., American Legion ceremonies)
- Protect the architectural character of the existing buildings that terminate important vistas, i.e., the former Suffern Hotel and Sacred Heart Church.
- Allow for the creation of additional murals to provide visual interest downtown. Themes could include area history, such as the early railroad, the Ramapo Pass, early businesses, the Suffern family.
- Protect stone pillared entrance at the vacant parcel on Wayne Avenue.

E. CIRCULATION AND PARKING

1. Pedestrian Circulation

- Improve sidewalks along Wayne Avenue.
- Provide decorative sidewalk along Lafayette Avenue and Orange Avenue in accordance with a streetscape plan.
- Enforce the prohibition of parking on sidewalks.
- Provide on-street parking along Hallett Place.

2. Vehicular Circulation

- Enforce the cleanup of garbage along Hallett Place.
- Study a village takeover of Hallett Place.

3. Curbing and Curb Cuts

- Provide controlled access to parking areas, and close curb cuts to the maximum extent practicable.
- Reconstruct sunken curbs along Wayne Avenue to control property access.
- Protect existing granite curbing.

4. Parking

- Screen sidewalk views into parking areas.
- Eliminate use of cyclone fencing within the Central Business zoning district, and provide landscaped screen where elsewhere provided.
- Add additional on-street parking spaces where curb cuts have been eliminated.
- Provide additional on-street parking on west side of Orange Avenue.

F. SECURITY

- Increase lighting along Lafayette Avenue in the vicinity of Sacred Heart Church.
- Provide pedestrian scaled street lighting throughout study area.

V. CONSERVATION DISTRICT PLAN

The Conservation District Plan is intended to provide guidance for the future development and redevelopment of properties, and to serve as the basis for a conservation district overlay zoning district. The proposed conservation district zoning overlay encompasses properties and public right-of-way to be governed in accordance with the regulations of the proposed zoning district, and the guidelines contained in this section.

A. CONSERVATION DISTRICT OVERLAY AND BOUNDARIES

This plan recommends the development of a conservation district overlay zone that would serve the following purposes:

- a. Require that new construction adhere to specific design guidelines that are part of the district requirements/standards based on the existing architectural and design features that are to be encouraged within the overlay zone.
- b. Ensure that alterations to existing buildings of local historic and architectural significance are reviewed by the Planning Board to ensure that significant features are preserved, and provide a mechanism for review of any applications for demolition of existing buildings of local historic or architectural significance.
- c. Prohibit parking as a use on private property directly fronting along Lafayette Avenue.
- d. Discourage new curb cuts along Lafayette Avenue.
- e. Require a minimum front yard requirement for new construction to ensure adequate sidewalk width within the overlay zone.
- f. Require a maximum front yard depth to encourage pedestrian activity along the street.
- g. Require street trees, sidewalk, lighting and similar improvements as part of new construction, or substantial rehabilitation, in accordance with a streetscape plan.
- h. Require screening of existing parking within the overlay zone.
- i. Serve as the basis for establishing a business improvement district within downtown Suffern.

The boundaries of the proposed conservation district overlay are illustrated in **Figure 8**. The boundary is based on the existing conditions analyses contained in Section II. The proposed conservation district overlay boundary coincides with the study area boundary illustrated in **Figure 1**.

Implementation of the conservation district overlay is discussed in Section VI., Implementation.

B. LAND USE

1. Conversion of Residential Uses to Commercial Uses

It is the intent of the conservation district plan to encourage retail, commercial, office and personal service businesses within the conservation district study area. Existing single and multi-family residences, and upper level residential uses within mixed use buildings, are encouraged to convert to nonresidential uses. The existing underlying zoning of properties within the proposed conservation district overlay has not permitted residential uses since the 1977 zoning law and the 1993 adopted zoning amendments.

2. Conversion of Auto-related Uses along Orange Avenue

Two establishments located at the southern terminus of Orange Avenue within the conservation district study area are non-conforming auto-related and fabrication uses within the Central Business zoning district. The location of these uses represents an important visual entry into the central business district. To provide a cohesive street environment for the conservation district, and enhance this visual entry into the village, sidewalk detailing, tree plantings and street lighting should commence in this location. However, due to the nature of these establishments, e.g., vehicular storage and uncontrolled access onto the properties due to the absence of curb cuts, it is difficult to provide these streetscape amenities in this important location. The Village should encourage the infill and conversion of these non-conforming uses to commercial uses.

C. ZONING

Parking will not be permitted within the front yard of any property that fronts on Lafayette Avenue in order to promote a visually attractive environment.

In order to provide adequate sidewalk width to accommodate pedestrians, street trees, and street lamps, new construction should be placed a minimum distance of 12 feet from the existing curb (2-foot decorative border and 10-foot paved sidewalk).

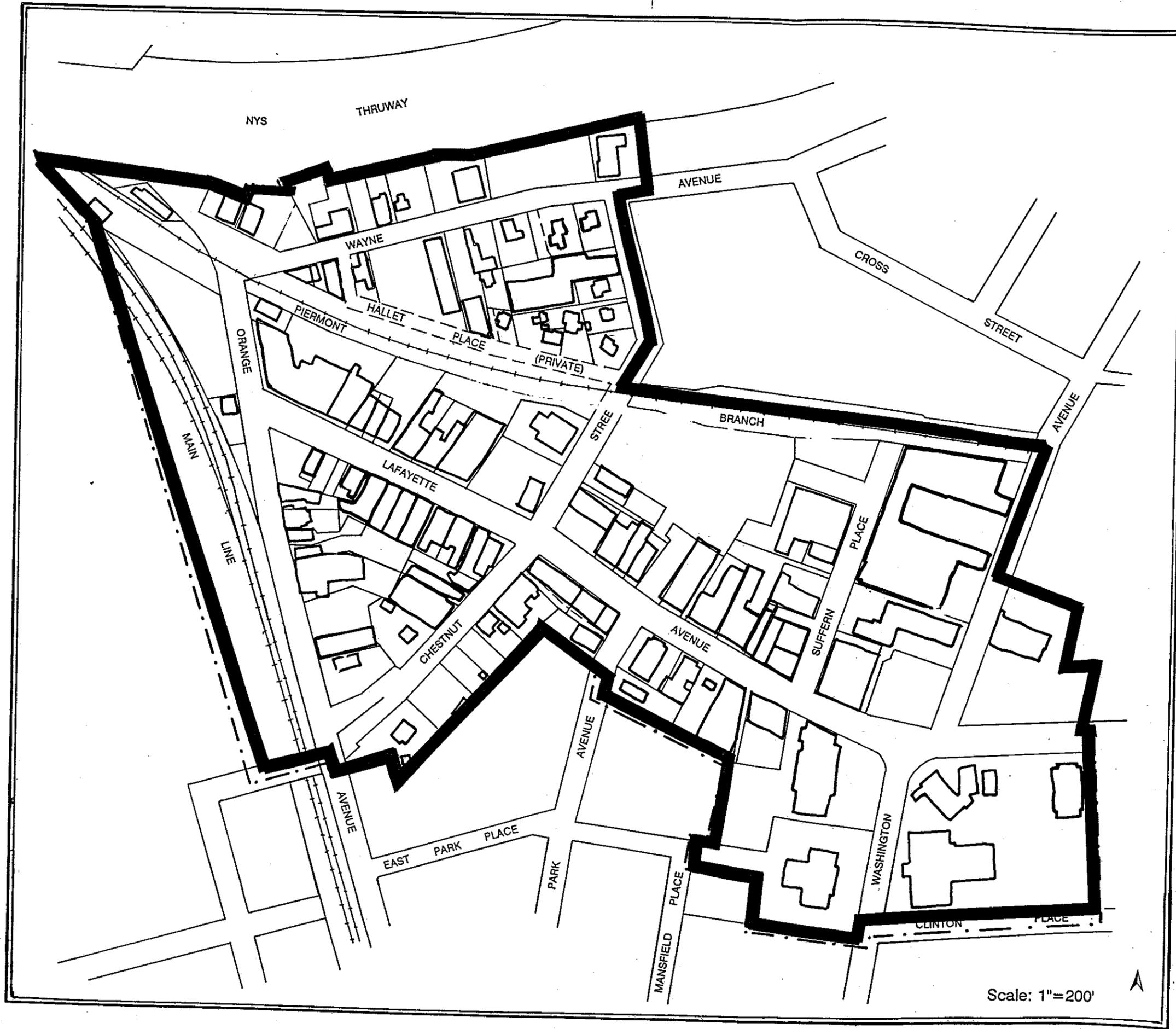


Figure No. 8
**PROPOSED
 CONSERVATION DISTRICT
 OVERLAY BOUNDARIES**

LEGEND

█ Proposed Boundary

VILLAGE OF SUFFERN
 * CONSERVATION DISTRICT *
 STUDY

D. PROPERTIES AND BUILDINGS

The conservation district study area contains buildings of local historic and architectural significance. The existing conditions survey identified specific architectural and historic features of buildings located within the conservation district study area. Buildings of Interest within the conservation district overlay area were previously identified in **Figure 5**, and are the subject properties intended to be protected, preserved and enhanced by these standards.

It is the goal of this conservation district plan to preserve the features and structures that lend the proposed conservation district overlay area its historic character and unique sense of place. This section of the Plan addresses the features to be preserved on existing buildings, and features that should be incorporated in new construction to ensure that new buildings are compatible with existing historic buildings dating to the turn of the century.

Overall, the conservation district overlay area represents an eclectic mix of building styles, reflecting the evolution of the downtown area. Therefore, the intent of this section is not to suggest a specific building style, but to identify key features that should be preserved, and suggest design guidelines that ensure that renovated and new development enhances the character of the downtown district.

1. Facade Materials

The buildings which lend the Village its sense of place include the Lafayette Theatre, Sacred Heart Church, the former Lafayette and Suffern Hotels, the Comesky Block, and numerous brick rowfronts identified in **Figure 5**.

The majority of these buildings exhibit the following facade materials:

- Patterned brick, in a variety of colors
- Stucco

Aluminum and other metallic surfaces would not be permitted as part of the building facade. However, these materials may be appropriate for use as cornices, moldings, or other decorative elements.

2. Roofline

Rooflines must be differentiated from the building facade. The use of tile roofs or decorative cornices is required (see **Figure 10**).

For single story buildings, parapet walls should have decorative detailing.

3. Architectural Details

The following architectural details must be preserved on existing buildings or included as part of new construction:

- stone, concrete or brick lintels
- stone, concrete or brick sills
- keystones
- decorative cornices and molding
- bracketed cornices
- the use of "timbering"
- providing name and/or construction date of building

These features should be emphasized, e.g., if a building is brick, then the lintels, sills, keystones should be in concrete or stone, and the color varied from the building's facade.

4. Window Treatment

Reflective (mirror) and ribbon (continuous horizontal belt course of glass not broken up by the building's facade) glass would not be permitted for new structures or the replacement of existing windows within the conservation district study area. Windows should be emphasized through the use of lintels, sills, and other decorative devices described previously. Arched windows are encouraged.

5. Building Entry

A building's entry must be attractive. Fanlights, arched windows, patterned brickwork, a pedimented doorway or similar treatment is required.

6. Building Height

Two- and three-story buildings should be encouraged within the conservation district study area. It is recommended that one-story buildings be designed with brick or stuccoed parapets that add height to the structure and provide a more consistent elevation within the conservation district overlay. Aluminum and other metallic surfaces are not permitted, e.g., Suffern Plaza.

Horizontal variation is required between the ground level and upper level of a building, either through the use of a cornice, change in building mass, or by providing patterned brickwork.

Figure 9 represents a photomontage of buildings exhibiting important design features. **Figure 10** provides a comparison of two buildings with and without the appropriate level of design detail.

E. STREETScape

Several recommendations of this Conservation District Plan are related to specific capital improvements that are intended to beautify and strengthen the quality of the conservation district overlay area aesthetically. A long-term commitment and investment in the study area's infrastructure will help to retain and further attract businesses, and will provide a quality environment for Suffern residents and patrons who visit the conservation district.

The Conservation District Plan recommends improvements that will enhance the overall sense of place within the study area. These infrastructure improvements include street lighting, tree and other plantings, signage, sidewalk details, and other improvements. These conceptual improvements require further engineering, cost analyses, and plan development to be effectuated. This Plan is intended to provide general guidance on the long-term implementation of Conservation District improvements.

1. Signs

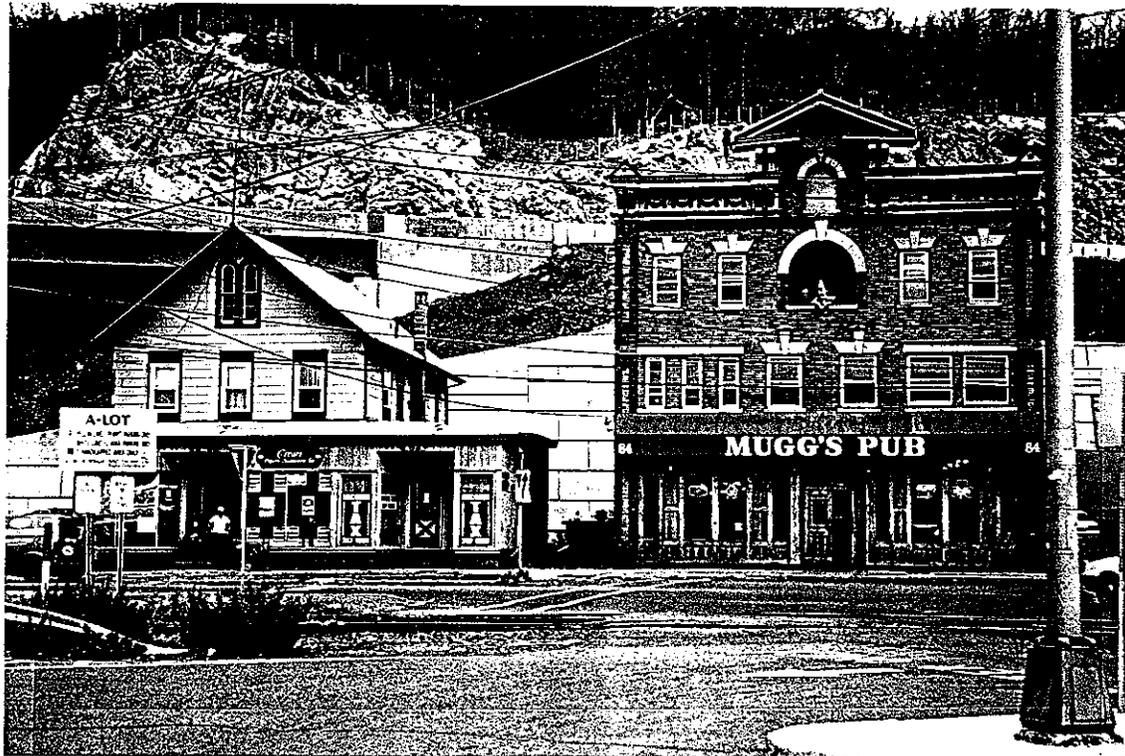
a. Awning/Canopy signs

Awning/canopy signs are not permitted sign types. However, existing awnings/canopies have been amortized. Several establishments within the study area utilize existing awnings/canopies for signs; when limited to a small area, e.g., above a doorway, these canopies do not impact a significant volume of sidewalk "space." However, several establishments, e.g., the furniture retailer in front of the Lafayette Theatre, maintain canopies that extend almost the length of the facade. The canopy, extending into the public right-of-way effectively reduces the amount and volume of space for streetscape amenities, e.g., the crowns of street trees.

The Conservation District Plan encourages the conversion of existing canopy signs to facade or window signs, particularly in locations where limited sidewalk width is available to accommodate streetscape improvements.



Sacred Heart Church terminates this important vista. Quoins, arched entrance, arched windows, brick facade, and dome all contribute to the character of this building.



The former Suffern Hotel terminates an important vista. Concrete sills, lintels with keystones, patterned brick and elaborate cornice and decorative molding are features to preserve. Note the number of temporary signs on the adjacent building which creates a cluttered appearance - the conservation district seeks to limit these signs.

**Figure 9
PHOTOS OF IMPORTANT DESIGN FEATURES**

VILLAGE OF SUFFERN
* **CONSERVATION DISTRICT** *
STUDY



A very handsome block in Suffern. Brickwork, stone/concrete sills, lintels with keystones, elaborate cornices lend the district overlay area its sense of place. The new signs at the right side of the picture, provided with the cooperation of the Chamber of Commerce, are restrained, while identifying the business inside, and do not detract from the visual appearance of the buildings themselves.



Decorative cornices and molding must be preserved. The size, location and typeface of this sign are (were) all appropriate to the location and style of the building.

Figure 9 (cont.)

PHOTOS OF IMPORTANT DESIGN FEATURES

VILLAGE OF SUFFERN

* CONSERVATION DISTRICT *

STUDY



Variation in massing, e.g., the projecting bay at the Shuart Building, provides visual interest.



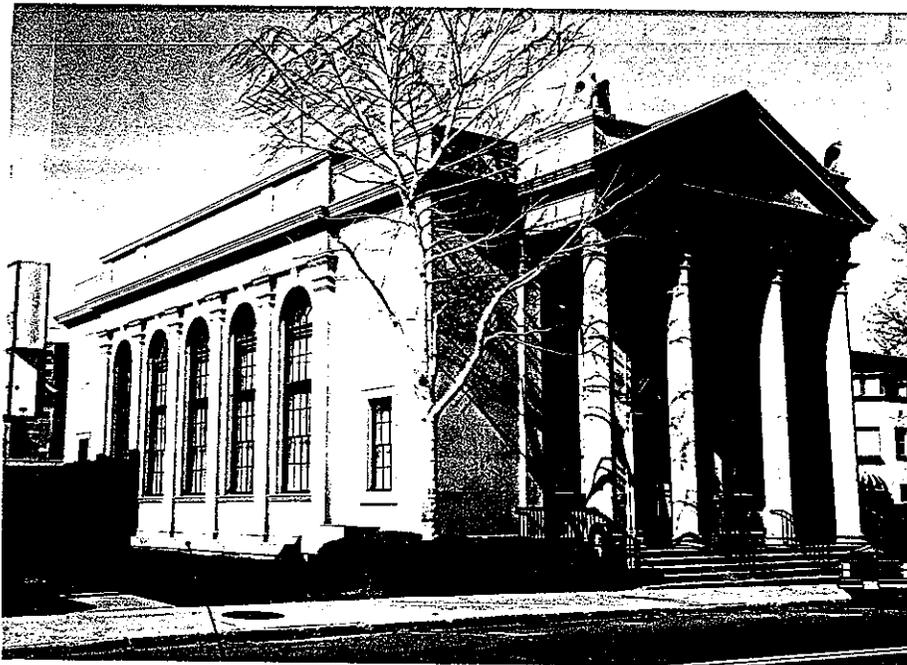
Note the cornice that provides horizontal variation between the ground floor and the upper story. Patterned brick and the cornice with decorative molding, including the building's construction date, provide visual and historical appeal.

Figure 9 (cont.)
PHOTOS OF IMPORTANT DESIGN FEATURES

VILLAGE OF SUFFERN
* **CONSERVATION DISTRICT** *
STUDY



The tiled roof, stucco, and "timbered" facade are common building features in the study area.



A monumental building in Suffern to be preserved.

Figure 9 (cont.)

PHOTOS OF IMPORTANT DESIGN FEATURES

VILLAGE OF SUFFERN

* CONSERVATION DISTRICT *

STUDY



Appropriate level of design detail: 2 stories, concrete lintel with keystone, brick sills, massing is broken up into smaller masses vertically and horizontally, entrance is defined by patterned brick arch and palladian window, appropriate fenestration (i.e., no ribbon glass), decorative roof.



Building on right has inappropriate level of detail

Figure 10

COMPARISON OF DESIGN DETAILS

VILLAGE OF SUFFERN

*** CONSERVATION DISTRICT ***

STUDY

Roll-top awnings, when properly maintained, would be permitted in the district overlay area.

b. Coordinate signage on multi-tenant buildings

Facade signs on multi-tenant buildings will be coordinated to provide a cohesive pattern of signage. Signs will be coordinated by sign size, color, and font type.

c. Coordinate signage with the colors and features of buildings

Signs will be coordinated with the architecture and colors of the buildings upon which the signs are to be located. For example, a sign box with modern lettering will be discouraged on a victorian, early 1900s building.

d. Enforce regulations pertaining to window signs

Paper window signs proliferate within the conservation district study area. These signs, intended to be temporary, are often permanent advertising devices. In addition, the total window area used to display these signs often exceeds the 10 percent permitted by the sign regulations. The Village should enforce the regulations pertaining to window signs.

e. Erect signs identifying municipal parking areas

A coordinated set of municipal signs should be erected directing patrons to municipal parking lots within the conservation district study area.

Access to municipal public parking should be clearly posted within the downtown area. Recommended locations for signage include the following:

- Intersection of Suffern Place with Lafayette Ave.
- Intersection of Chestnut Street with Lafayette Ave.
- Intersection of alley entrance between the Miranda Building/Lafayette Theatre and Lafayette Ave.

f. Rehabilitate or remove unutilized sign supports

Two abandoned sign supports are located at the auto establishment on the east side of Orange Avenue at the southern end of the conservation district overlay area. These signs detract visually from this important visual entry into the district and should be rehabilitated or removed.

g. Provide visual interest by allowing icon signs

Icon signs are "representational" signs that identify the use of an establishment, e.g., a pair of glasses advertising an optometrist, or a barber pole identifying a barber shop. These types of signs can provide visual interest within a community, and should be encouraged provided the sign meets all other sign regulations.

2. Street Furniture

Street furniture recommendations are limited to providing a consistent design of waste receptacles in the conservation district study area. **Figure 11** provides the recommended design.

**FIGURE 11
WASTE RECEPTACLE**



**Manufactured by
Victor Stanley, Inc.**

Model S-42: With optional S-2 spun steel dome.

3. Street Trees and Plantings

Cornell Cooperative Extension conducted a street tree inventory to identify existing street trees within the study area and assess their condition. Based on the results of this inventory, another field survey was conducted to recommend additional plantings within the conservation district. The recommendations are based on the following:

- existing sidewalk width
- tree species in the vicinity
- existing building height adjacent to sidewalk
- visual interest
- maintenance, e.g., fruit bearing, blossoms, etc.

In addition, recommendations were made related to the potential location of these trees, tree spacing, tree wells, grates and other protective devices. The recommendations are provided in this section.

a. Sidewalk Widths

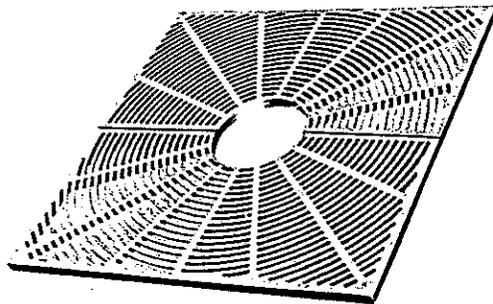
Based on a field survey conducted with Mr. Paul Trader of the Cornell Cooperative Extension, it is recommended that sidewalks with widths of eight (8) feet and greater be planted with street trees within the district overlay area. Sidewalks with widths of less than eight (8) feet cannot accommodate street trees without potential disruption to the pedestrian environment. In locations where sidewalk widths do not permit street tree plantings, it is recommended that decorative planting boxes be positioned adjacent to storefronts.

b. Tree Protection

In order to protect the Village's investment in street trees, some protection is required to ensure that the trees are not damaged by pedestrian or vehicular traffic. It is recommended that the Village install tree grates and tree guards to protect street trees from potential hazards.

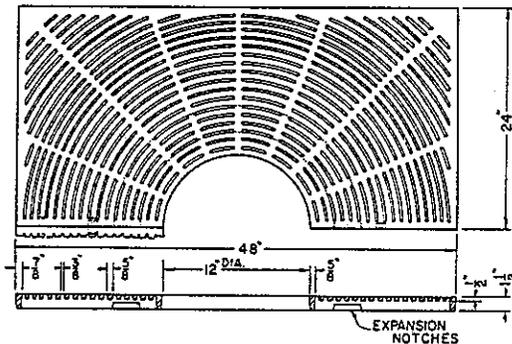
Tree grates should be a minimum of 4' x 4', with narrow slot openings so that pedestrians can walk over the grates if necessary. This is particularly important given the limited sidewalk widths in some locations. **Figure 12** provides a recommended standard.

R-8710 180° SQUARE



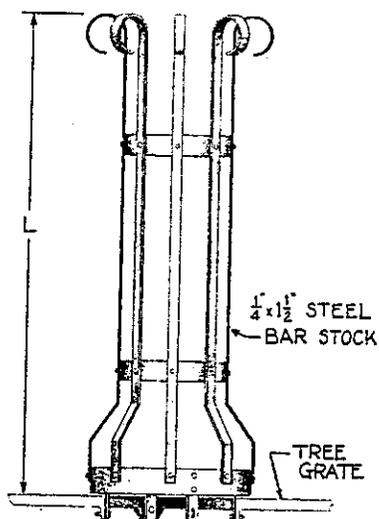
Note $\frac{3}{8}$ " slot openings for special pedestrian requirements. Grate is notched in underside ribs for ease of expanding tree opening to 18" and 24". Available with cast iron angle frame, if required.

Weight per set - 290 pounds.



HALF PLAN AND SECTION

**Tree Grate
Neenah Foundry Co.**



STYLE E

**Tree Guard
Neenah Foundry Co.**

**Figure 12
PROTECTIVE DEVICES FOR STREET TREES**

VILLAGE OF SUFFERN
* CONSERVATION DISTRICT *
STUDY

Tree guards should also be installed to protect tree trunks from vehicles potentially backing into street trees while parallel parking. These guards have a minimum 12" diameter. **Figure 12** provides the recommended standard.

c. Location of Tree Plantings

The exact location of tree plantings must be determined in the field at the time of planting. However, the following guidelines apply:

- (1) To permit easy access by the fire department, there should be an unencumbered access corridor to any building entry (including fire escapes, stairs, etc.).
- (2) Trees shall be located to provide adequate space between the tree and public street lighting.
- (3) Sidewalks must be maintained with a minimum width of six (6) feet between tree trunks and building faces, stoops, railings etc.
- (4) A clear zone unobstructed by trees shall occur at each intersection. The intent of this clear zone is to give full visibility of the intersection. The following rules shall apply:
 - No trees shall be planted within 40' of the right-of-way along any public street.
 - No trees shall be planted within 10' of a driveway/curbcut onto a public street.
- (5) In order to maintain unobstructed views of street signs, trees must be branched up to 7'-0". New York State DOT requires 260' of unobstructed views of street signs. Other lateral utility lines must not be encroached upon by tree pits.
- (6) Fire hydrants require a 5' circumference to be clear of all three dimensional encumbrances including street tree trunks.
- (7) Wherever possible, all lateral utility lines, i.e., gas, water, sanitary, storm, etc., should have 4'-0" clear of tree trunks to each side of the utility line.
- (8) Tree pits shall be 4'-0" in width and may be contiguous with the curb. All tree pits shall be 3'-0" deep.

- (9) Standard measurements for tree caliper and minimum ball size shall conform to the American Association of Nurserymen.
- (10) Recommended spacing is 25-40 feet between trees, depending upon specimen size.

Street trees will be planted in the general locations illustrated in **Figure 13**. **Table 3** provides a description of tree planting recommendations according to street segment.

**TABLE 3
TREE PLANTING PLAN**

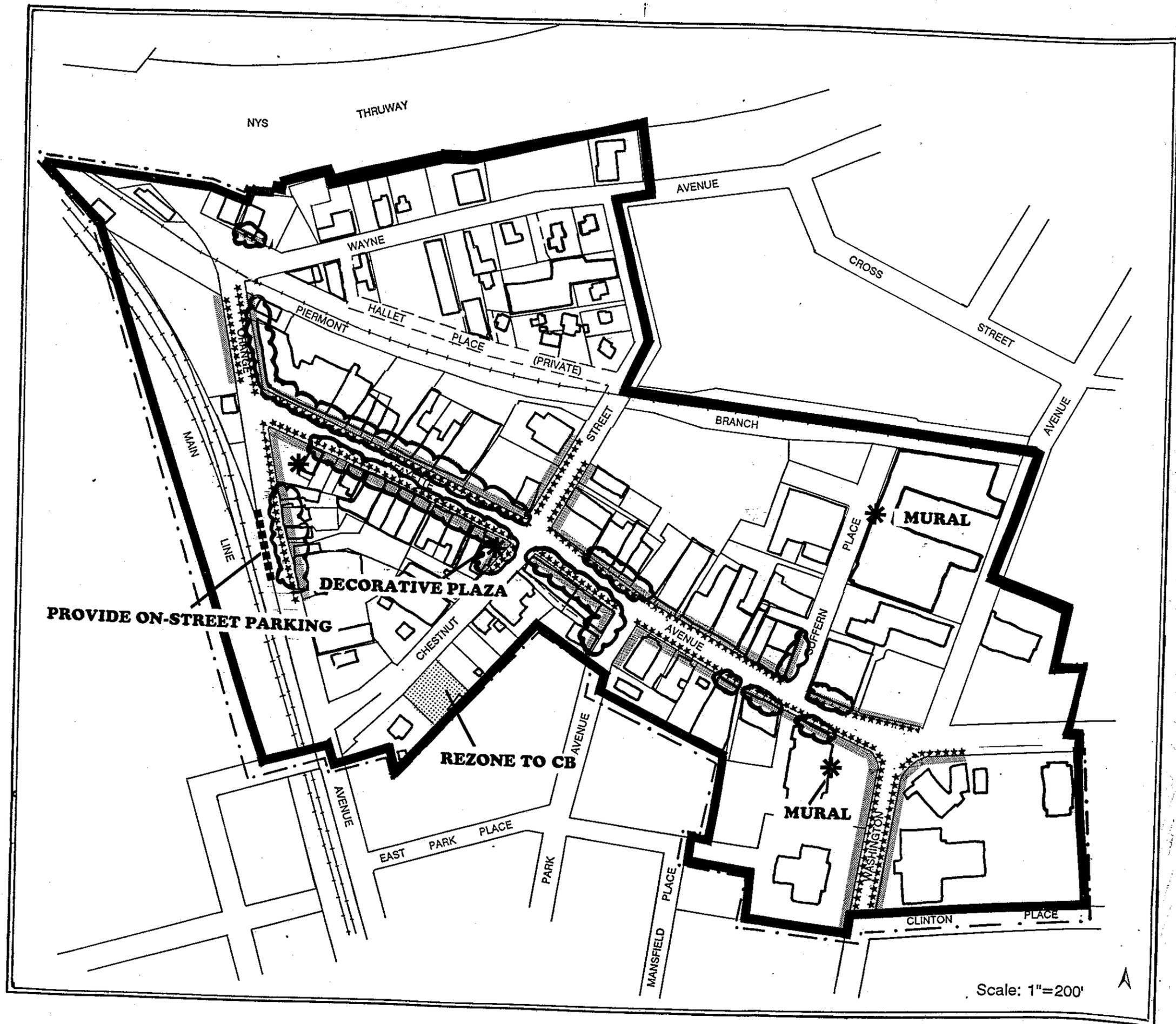
Street	Segment	Side of Street	Streetscape Characteristics	Recommended Plantings
Lafayette Ave.	Washington Ave.- Suffern Pl.	North	Sidewalk width in front of Suffern Furniture is 9.5 feet. However, canopy reduces the volume of space necessary for the tree canopy. Begin plantings west of Avon Park.	Provide two (2) trees of same specimen. Columnar Sargent Cherry is recommended.
		South	From Lafayette Theatre parking lot to alley entrance. Sidewalks approximately 8.5-10 feet. Theatre parking area contains treed planting strip. Marquis in front of theatre reduces space for trees.	Provide two (2) trees on either side of marquis, potentially across from trees on north side of street, and same specimen. Columnar Sargent Cherry is recommended.
Lafayette Ave.	Suffern Pl.- Park Ave.	North	Sidewalk widths from the intersection to Suffern Plaza approximately 7.5 feet. This is too narrow to accommodate street trees and not negatively impact pedestrian corridor.	No street trees recommended.
		South	Sidewalk widths from the Miranda Building to the Bank of New York range from 10 feet to 7.5 feet. Three pin oaks are located in front of Miranda Building: two should be removed and replaced with columnar forms. In front of Casa del Sol, could accommodate one street tree. Beyond this establishment, sidewalks are narrow. Trees on bank properties provide greenery, and are adequate.	Pin oaks are not appropriate street trees and should be removed. Replace with Japanese tree lilac.
Lafayette Ave.	Park Pl. - Chestnut St.	North	Sidewalk width from vacant lot to drive-in associated with Marine Midland adequate to accommodate street trees. Approximately 160' of frontage. No addition street trees required along sidewalk in front of Marine Midland Plaza.	Provide approximately four (4) Princeton Upright Scholar trees.

		South	Sidewalk width approximately 10.5 feet in width - adequate for street trees. Two existing Norway maples in front of bakery and florist are dead and in poor condition, respectively. Approximately 200 feet of frontage.	Remove two Norway maples. Provide approximately five (5) Chanticleer Callery Pear trees.
Lafayette Ave.	Chestnut St. - Orange Ave.	North	Sidewalks range in width from 10-10.5 feet. Approximately 600 feet of frontage, only broken up by drive-through associated with North Fork Bank. Two Norway maples and a red maple in the vicinity of North Fork Bank and the Indian restaurant are in fair to poor condition. Should be removed. Tree plantings must take into consideration NYS DOT sign at the southeast corner of Comesky Block building.	Remove two trees, Red maple and Norway maple, in poor condition. Replant with Japanese Zelkova (Village Green variety). Provide two (2) additional Zelkovas, one in front of Creative Universal studios, and one in front of Physical Grafitti. Provide approximately seven (7) Chanticleer Callery Pear trees east of NYS DOT sign.
		South	Approximately 460 feet of frontage from Chestnut St. intersection to northwest corner of building adjacent to park, broken up by two curb cuts. Sidewalk widths range from 9.5-10'. Four (4) existing trees; one maple and three Norway maple. Only the Norway maple in front of drycleaners is in good condition. Do not need to provide street trees in front of park.	Remove maple and two Norway maple. Provide approximately 10 street trees; Zelkova's should be planted across from the Zelkova's on the north side; then plant Chanticleer Callery Pear across from those planted on the north side and end at the park.
Wayne Ave.	West of Orange Ave.	North	NYS DOT will create an island in front of Muggs Pub as a result of intersection improvements. There are currently no trees in this vicinity. This location is an important visual termination point for Orange Avenue.	Provide 2-3 street trees. Plaza will allow large canopies. Use Japanese Zelkova.
		South	Inadequate sidewalk width, the rail right-of-way and the New York State Thruway right-of-way are a hostile environment for trees.	No plantings recommended.
Orange Ave.	Wayne Ave. - Lafayette Ave.	West	Sidewalk, where it exists, is inadequate to accommodate street trees.	No street tree plantings recommended.
		East	Approximately 9.5 feet of street width in front of Comesky Block. Existing NYS DOT sign limits location of tree plantings. Approximately 200 feet of frontage. No parallel parking along this portion of Orange Avenue.	Provide approximately five (5) Oakleaf Mountain Ash.
Orange Ave.	Lafayette Ave. - Chestnut St.	West	West side of Orange Avenue accommodates parking or is within the rail right-of-way.	No street tree plantings recommended.

		East	Sidewalk width is approximately 10.5 feet. No street trees necessary in front of existing park. Approximately 460 feet frontage (exclusive of park). Along south end of block, existing automotive repair establishment, fabricator and Mountain Valley Brew Pub (MVBP) parking area have multiple curb cuts making it difficult to plant street trees. NYSDOT sign is also located near northwest corner of MVBP constrain tree plantings.	Provide one (1) Accolade Flowering Cherry in front of deli adjacent to MVBP. Provide two (2) Accolade Flowering Cherry in front of gallery and Moroneys. Provide one (1) Accolade Flowering Cherry in front of antique shop.
Washington Ave.	South of Lafayette Ave.	West	Existing trees within landscape strip buffering theatre parking lot. Also mature street trees are located in front of Village Hall.	No street tree plantings recommended.
		East	Existing mature trees associated with Sacred Heart Church property.	No street tree plantings recommended.
Suffern Pl.	North of Lafayette Ave.	West	Sidewalk width ranges from 2.75-8 feet. Very little pedestrian activity. Existing Norway Maple (poor condition) in front of Brite Star and Ameriloan building.	Replace Norway maple with Japanese tree lilac. Plant one (1) Japanese tree lilac alongside the Gift Cupboard.
		East	Sidewalk widths vary between 5-7.5 feet. Inadequate width for street trees.	No street tree plantings recommended.
Park Pl.	South of Lafayette Ave.	West	Sidewalk width approximately 9.5 feet. One curb cut between Park West salon and bakery.	Plant three (3) Japanese tree lilacs, one in front of salon, and two in front of bakery.
		East	Sidewalk width is approximately 7.5 feet. Could be adequate width for street trees because of limited pedestrian activity, however, existing landscaping associated with Bank of New York building is adequate.	No street tree plantings recommended.
Chestnut St.	Piermont Branch ROW - Lafayette Ave.	West	Sidewalk width ranges between 6.5-7 feet. Inadequate width for street trees. Limited pedestrian activity.	No street tree plantings recommended.
		East	Sidewalk width ranges between 5-6.5 feet. Inadequate width for street trees. Limited pedestrian activity.	No street tree plantings recommended.
Chestnut St.	Lafayette Ave. - Orange Ave.	West	Sidewalk width ranges between 6-7 feet. Inadequate width for street trees.	No street tree plantings recommended.
		East	Sidewalk width is approximately 6 feet. Inadequate width for street trees.	No street tree plantings recommended.

Where sidewalk width does not permit street tree plantings, planters should be placed within the first floor windows of each establishment. The planters could be designed similarly to those in front of Moroney's Pub, located along Orange Avenue. These planters are wooden, located beneath the windows of the establishment, and have been painted in the same color as the facade. In order to provide a consistent design, the planters should be handmade in order to fit under the variously sized windows located throughout the downtown. The Chamber of Commerce could sponsor a vocational project with BOCES, Rockland County Community College, or Suffern High School, to develop specifications, design, and construct the planters for businesses within the conservation district area.

Figure No. 13
STREETSCAPE PLAN



LEGEND

- ***** Decorative Lighting
- ▨ Decorative Pavers
- ☁ Street Trees (refer to Table 3)

VILLAGE OF SUFFERN
 * CONSERVATION DISTRICT *
 STUDY

4. Lighting

It is a recommendation of this District Plan to provide pedestrian-scaled street lamps within the conservation district overlay area.

Figure 6 illustrates the location of existing lighting fixtures. Representatives at Orange and Rockland Utilities, Inc. (O and R) and the Village Department of Public Works do not maintain plans identifying the location of wiring for the electrical fixtures. The lack of a lighting plan has become a recurring problem for maintaining and improving electrical fixtures within the Village's downtown area. As a result, any changes to existing lighting fixtures within the Village would have to be preceded by an identification of the exact location of wiring for the fixtures.

Generally, existing fixtures within the study area are located on one side of the street. It is assumed that wiring will have to be extended across the street to provide decorative street lamps on both sides of a street.

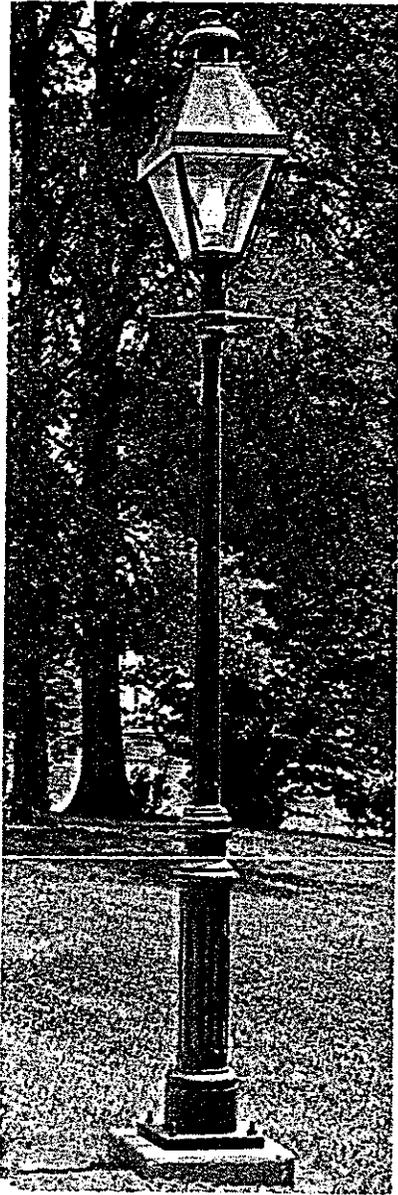
Cast iron lighting posts are recommended for installation. The use of aluminum posts is rejected because: (1) aluminum is a softer material than cast iron which can be damaged by vandals; (2) aluminum may transmit vibrations from street traffic; and (3) cars backing into on-street parking spaces may dent the base or move the column out of vertical position. Lastly, the cost differential between aluminum and cast iron posts is not significant, although cast iron is slightly more expensive.

The recommended fixture is consistent with the luminaires found at the Washington Avenue war monument, Avon Park and Village Hall. **Figure 14** illustrates the recommended luminaire and its post. **Figure 13** identifies the recommended street location of luminaires within the conservation district. The exact location of street lighting should be determined prior to installation.

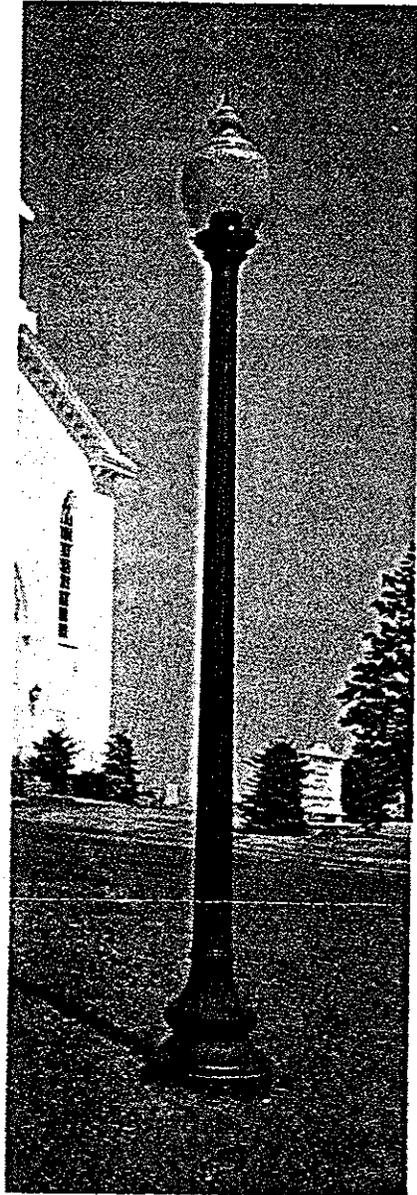
The following guidelines apply:

- Lighting should be installed one (1) foot from the inside curbface. This would center the fixture within the two (2) foot decorative paver border provided along Lafayette and Orange Avenue (refer to Section V.F.I.a).
- The spacing of lighting should be determined in concert with the decision as to whether existing lighting should be retained.

O and R has no existing administrative mechanism to assist the Village with the installation of decorative street lamps.



HANCOCK



VILLA

Decorative Street Lamps: Villa Luminaire with Hancock Cast-Iron Post. Post is 12 feet with 10.5 inch base (excluding luminaire).

Spring City Electrical Mfg. Co.

Figure 14

DECORATIVE STREET LAMPS

VILLAGE OF SUFFERN

*** CONSERVATION DISTRICT *
STUDY**

The Village will likely bear the cost of installing the new posts. However, further discussion with O and R is warranted, since the utility assisted the Borough of Allendale with the replacement of lighting within that community's business district.

5. Plazas

The Conservation District Plan recommends the improvement of the vacant, macadam surfaced plaza located at the southwest quadrant of the intersection of Chestnut Street and Lafayette Avenue. Improvement of the plaza would include the following:

- decorative paving blocks
- decorative lighting consistent with street lamp standard
- landscape plantings adjacent to the drycleaning building

Specific design of these improvements would precede improvement of this plaza.

6. Other Visual Features

a. Murals

The Village should pursue public arts projects, including the creation of a mural on the following blank facades:

- Lafayette Theatre wall facing onto theatre parking lot and Washington Avenue
- Avon Warehouse building facade fronting on Suffern Place

b. Existing War Monument

The existing war monument located at the intersection of Washington Avenue and Lafayette Avenue is an important feature of the conservation district overlay area; all activities, particularly street improvements, occurring within this vicinity will be reviewed and denied or recommended for denial, if appropriate, if the monument is negatively impacted by the proposed activity.

c. Community Bulletin Board

A community bulletin board is proposed in either one of two highly visible locations: Within the planting strip adjacent to the Lafayette Theatre parking lot at the intersection of Washington and Lafayette Avenue; or within Avon Park.

F. CIRCULATION AND PARKING

Improved circulation and parking will retain the viability of the conservation district study area.

1. Pedestrian Circulation

a. Sidewalks

New wiring must be installed in order to accommodate decorative street lamps. Because the sidewalk will be disrupted in order to install these fixtures, in addition to disruptions to the sidewalk to install tree grates, it is recommended that the Village resurface a two-foot border adjacent to the inside curb face of the sidewalk with decorative pavers.

These pavers would also extend across driveways. The use of bricks are discouraged because of their tendency to chip from vehicular usage across driveways. The exact manufacturer, design and color of the pavers must be determined prior to installation. However, the following is recommended:

- The pavers should be patterned to provide visual interest. This can be accomplished by using different pavers sizes. One example is Anchor Concrete Products, Inc., Classico pavers
- The variation in building styles and colors should be mimicked in the pavers, i.e., provide a blend of colors instead of a single color. Anchor Concrete Products, Inc., Flash Brown and Sable Blend is an attractive mix of brown, tan, charcoal, light gray and dark gray.

Unilock pavers or other manufacturers of pavers are also acceptable if their products provide the desired visual effect. Drop curbs at crosswalks within the study area must be reconstructed at the time the pavers are installed. Crosswalks should be restriped.

The Village should explore with the NYSDOT the opportunity to repair and replace existing sidewalks along Wayne Avenue from its intersection with Orange Avenue east to Washington Avenue. Landscaping in front of Muggs Pub should also be reviewed and approved by the Village prior to design finalization.

2. Vehicular Circulation

There are no recommendations related to vehicular circulation.

3. Curbing and Curb Cuts

The opening of new curb cuts, particularly along Lafayette Avenue, should not be permitted if existing alternative access is available. Existing curb cuts should be consolidated, with single entrypoints providing access to rear parking locations. These two measures will benefit the pedestrian circulation system.

Granite curbing should be retained and preserved - during recurbing, the Village should attempt to replace concrete curbing with surplus granite curbing along Lafayette Avenue.

4. Parking

a. On-Street Parking along Orange Avenue

During numerous field investigations, vehicles were observed parking at all times of the day along the west side of Orange Avenue, south of this avenue's intersection with Lafayette Avenue. It is recommended that the Village pursue the provision of on-street parallel parking in this vicinity.

b. Public Parking Signage

Access to municipal public parking should be clearly posted within the study area. Recommended locations for signage include the following:

- Intersection of Suffern Place with Lafayette Ave.
- Intersection of Chestnut Street with Lafayette Ave.
- Intersection of alley entrance between the Miranda Building and Lafayette Theatre and Lafayette Avenue

Signs could be attached to decorative street lamps.

G. SECURITY

The Conservation District Plan encompasses recommendations regarding security in so far as certain capital and design improvements may provide a greater sense of security to residents and visitors to the conservation district study area. These issues are related solely to nighttime activities; it would appear that there is an overall level of satisfaction with security during daytime hours.

Activities and visitations during the nighttime are generated primarily by the Lafayette Theater and restaurants that line Lafayette and Orange Avenues. However, it would appear that several gift shops and galleries have begun operation recently that could draw from the restaurant patrons. In order for these visitors to extend their visit and walk to other establishments in the study area, adequate lighting must be provided to achieve the appropriate sense of security. The Conservation District Plan recommends the following:

- ensure that all public rights-of-way are adequately lit;
- charge a Village agency/officer with the responsibility of surveying luminaires within the conservation district study area, and coordinating with Orange & Rockland Electric to maintain existing fixtures;
- pursue capital improvement projects to establish pedestrian-scaled lighting along Lafayette and Orange Avenues; and
- as part of a site plan review and approval for non-residential uses within the conservation district area, require applicants to provide external illumination.

VI. IMPLEMENTATION

In order to effectuate the recommendations and improvements described in the Conservation District Plan, the following implementation measures should be pursued.

A. ADOPTION OF THE CONSERVATION DISTRICT PLAN AS A PART OF THE MASTER PLAN

It is recommended that the Planning Board recommend, and the Village Board adopt, the Conservation District Plan as an element of the Development Plan: Village of Suffern, adopted May 1974.

Section §7-704 of the Village Law states that "Such [zoning] regulations shall be made in accordance with a comprehensive plan..." Because it is the intent of the Village to create a Conservation District Overlay, it is imperative that the Village Board, upon the recommendation of the Planning Board, adopt this Plan in order to implement the recommendations of this Plan.

B. CONSERVATION DISTRICT OVERLAY ZONING

It is recommended the Conservation District Study Area be adopted as a Conservation District Overlay in order to effectuate the implementation measures described in Section IV.A., Conservation District Overlay and Boundaries.

The Conservation District Overlay (CD-O) would be coterminous with the boundaries identified in **Figure 8**. The overlay is superimposed over existing zoning districts, and its regulations are in addition, or supercede, those contained in the underlying zoning.

The Conservation District Overlay zoning district should contain the following:

- set of purposes guiding future private and public improvements and development within the CD-O
- set of additional standards governing the improvements and development within the CD-O
- methods of ensuring the protection of structures and their architectural features identified in Figure 5, Buildings of Interest

- methods to ensure that new construction is compatible with existing buildings and enhances the CD-O's sense of place

These regulations and standards would be consistent with the policy recommendations contained in this Conservation District Plan.

C. BUSINESS IMPROVEMENT DISTRICT

Article 19-A of the General Municipal Law governs the creation of Business Improvement Districts (BID) in New York State. This legislation allows the imposition of an additional tax, or assessment, on property owners within a designated area in addition to the regular real property tax. A majority of the owners must agree to the establishment of the BID, as described in more detail below. The first purpose is to provide for district improvements located on, or within , the district which will restore or promote business activity. The improvements authorized by legislation include, by way of illustration:

1. construction and installation of landscaping, planting, and park areas;
2. construction of lighting and heating facilities;
3. construction of physically aesthetic and decorative safety fixtures, equipment and facilities;
4. construction of improvements to enhance security of persons and property within the district;
5. construction of pedestrian overpasses and underpasses and connections between buildings;
6. closing, opening, widening or narrowing of existing streets;
7. construction of ramps, sidewalks, plazas, and pedestrian malls;
8. rehabilitation or removal of existing structures as required;
9. removal and relocation of utilities and vaults as required;
10. construction of a parking lot and parking garage facilities; and
11. construction of fixtures, equipment, facilities and appurtenances as may enhance the movement, convenience, and enjoyment of the public and be of economic benefit to surrounding properties such as: bus stop shelters, benches, street furniture, booths, kiosks, display cases, exhibits, signs, receptacles, canopies, pedestrian shelters and fountains.

The second purpose for which money may be expended is to provide for the operation and maintenance of any district improvement listed above. The services in this category include:

1. enhanced sanitation services;
2. services promoting and advertising activities within the district;
3. marketing education for businesses within the district;
4. decorations and lighting for seasonal and holiday purposes;
5. services to enhance the security of persons and property within the district.

District expenses are financed by the benefited property owners. A district plan must set out the expenses to be incurred and identify how those expenses are to be financed. The properties in the district may be assessed to provide for the improvements or services, or the Village may issue bonds or other municipal obligations for the purpose of providing funds for capital improvements. Principal and interest payments on these bonds are financed from the proceeds of charges imposed upon the benefited real property within the district.

The process for establishing a BID is the following:

1. The adoption by the Village Board of a local law, subject to permissive referendum, providing that the BID's law will be applicable to the municipality.
2. If 30 days has passed since the adoption of the local law and a petition has not been filed requesting a referendum, the Village Board may then adopt a resolution authorizing the preparation of a district plan for the business improvement district. The Village Board may adopt this resolution upon: its own motion; upon request of the mayor; or by the written petition of the owners of at least 51% of the assessed valuation of all taxable real property within the boundaries of the proposed BID and at least 51% of the owners of real property within the area included in the proposed BID. A district plan is then prepared, outlining the proposed boundaries of the BID, the services and improvements to be implemented, a description of the budget and method of assessment, and the management structure for the BID.
3. The district plan is filed in the Village Clerk's office and the establishment of the district is based on the plan. The Village Board may then adopt a resolution which must contain:
 - a. a copy of the plan;
 - b. notice that a district plan is on file in the Village Clerk's office and any other place where the plan may be inspected;
 - c. the time and place where the Village Board will meet and hold a public hearing;
 - d. a statement that any owner of real property within the proposed BID, must file any written objections in the office of the Village Clerk within 30 days upon conclusion of the public hearing. The resolution must state that the BID will not be established if written objections from the following people are filed:
 - the owners of at least 51% of the assessed valuation of all real property within the proposed BID, or

- at least 51% of the owners of real property within the proposed BID
- 4. The resolution must be published in the official Village newspaper, and a copy mailed to each owner within the proposed BID.
- 5. The public hearing is held.
- 6. Thirty (30) days after the last public hearing, the Village Board must determine whether the proper number of objections have been filed. If they were not filed, then the Village Board may disapprove, further hold hearings, or approve the plan. The Village Board then adopts a local law approving the establishment of the district. The State Comptroller must review the plan before it becomes effective. The main purpose of the Comptroller's review is to ensure that the Village's tax and debt limitations will not be exceeded by the establishment of the district.

Each district must establish a district management association (DMA) which will be responsible for carrying out the activities described in the plan. The DMA must be incorporated under the not-for-profit corporation law. The DMA's by-laws or certification of incorporation must provide for voting representation by owners of property and tenants within the district. The board of directors of the DMA must consist of representatives of owners and tenants in the BID. Property owners must make up a majority of the board of directors. The tenant representatives must include tenants of commercial space as well as residential units. The three incorporators will also sit on the board. One incorporator would be selected by each of the following individuals: the mayor; the chief financial officer; and a Village Board member.

The BID would represent a cooperative partnership between the Village and its business community. Many of the improvements outlined in the Conservation District Plan may be financed in whole or in part by the revenues of the BID.

In terms of management structure, the existing Chamber of Commerce already funds many of the activities which could be funded through a BID. The Chamber's membership or officers could serve as a basis for establishing membership on the BID's board of directors.

D. CAPITAL IMPROVEMENT PROGRAM

In accordance with §99-g of the General Municipal Law, a municipality may undertake the planning and execution of a capital program. A capital program is defined as a plan of capital projects proposed to be undertaken during a six-year period, the estimated cost of the improvements, and the proposed method of financing. A tentative budget is prepared in accordance with the projects to be undertaken. For each project, a description of the following is required:

- description of project and total cost
- method of financing, including a breakdown of the sources of the finances
- an estimate of the impact of the project on the municipality's operating budget for three fiscal years following the project's completion

If the Village is to assist in the undertaking of infrastructure and streetscape improvements described in the Conservation District Plan, these improvements should be structured into the overall fiscal program of the Village. By long-term planning and financing of the improvements, it reduces the impact of the cost of a new project on any single fiscal year.

In addition, with a clear itinerary and prioritization of projects, the Village can also seek funds for any improvements from various county, state, regional, federal and other funding sources.

E. LOCAL HISTORIC DISTRICT

A clustering of several properties and structures are of historic significance in the Village, and should be protected through designation as either an historic district, or as individual historic landmarks, in accordance with §266-19 of the Village of Suffern Zoning Law. These properties are the following:

- Suffern war monument located at the intersection of Lafayette Avenue and Washington Avenue
- Sacred Heart Church
- Village Hall
- Lafayette Theater

VII. FUNDING

A number of sources provide monies to fund the improvements outlined in the Conservation District Plan. It is important for the Village to broaden its base of financial support for these undertakings so that the burden does not fall solely upon the Village or its business community.

A. NEW YORK STATE URBAN DEVELOPMENT CORPORATION

1. Commercial Revitalization Program

The New York State Urban Development Corporation's Commercial Revitalization Program assists communities in revitalizing the physical and economic conditions of downtown commercial districts, especially in areas weakened by physical blight, socioeconomic distress or economic deterioration.

The Program enables communities to provide grants to property owners for facade improvements to commercial properties located in neighborhood commercial strips or central business districts. The program also provides technical assistance grants to defray the costs of architectural design and supervision for each project.

The following entities may apply to the program:

- a district management association for a business improvement district;
- a not-for-profit corporation
- a joint venture between or among any of the above

UDC's construction grant assistance, in the form of matching grants, may be provided for the following non-structural, exterior improvements to commercial properties:

- selected masonry and wall repairs
- cleaning and repainting
- new awnings and signage
- window repairs or replacements including storm windows
- wall repairs, including decorative details and other design features
- storefront door and window repairs or replacement
- exterior stairs, stoops, doors;
- security grills/rolldown gates;
- street front lighting (signage and building)
- repair or replacement of visible roofing; and
- miscellaneous building facade features.

UDC will also provide each selected project with a technical assistance grant to defray the fees and expenses of a project architect who is retained to plan and design eligible building facade improvements.

UDC will fund up to 100% of the architect's fees, and up to 50% of the eligible project costs for each participating storefront (the owner/tenant must provide the other 50%), or up to 50% of the total eligible project costs.

2. Business Improvement District Development Program

The BID Development Program is a new program administered by NYSUDC. Funds would be potentially available to Suffern in the following two categories: BID Planning and Start-Up Grants. Phase I, Bid Planning, provides funds for costs associated with planning for a BID. These include staff, supplies, facilities, and consultant costs for meetings and public outreach, determining BID boundaries and assessment formulae, and incorporation and other BID development needs. Phase II, BID Start-Up, provides funds only to approved district management associations for organizational development, implementing services and strategic planning. Start-Up grants are intended to support approved BIDs prior to receipt of their first assessment monies.

3. Commercial District Revolving Loan Fund

This program allows existing BIDs to apply for funding to stimulate the development of central business districts and neighborhood commercial strips. Selected organizations will receive a one-time \$25,000 start-up grant in addition to approximately \$100,000 to capitalize a revolving loan fund.

B. NYS COUNCIL ON THE ARTS

The New York State Council on the Arts provides assistance for a wide range of projects and activities. These include:

- **Architecture, Planning and Design:** The program supports studies in the fields of architecture, landscape architecture, urban and rural planning, urban design, and historic preservation.
- **Capital Aid:** The Capital Program makes funds available for the improvement, expansion or rehabilitation of existing buildings owned or leased by nonprofit cultural institutions that receive programmatic funding from the Council, including theatres, museums, galleries, offices, art storage/conservation, dance studios and historic buildings.

C. PRESERVATION LEAGUE

The rural New York Historic Preservation Grant Program is administered by the Preservation League of New York and makes grants to aid in the preservation of the architectural heritage of rural New York. The program has in the past awarded grants to small villages in suburban environments.

Eligible project categories include:

- Conditions, feasibility and rehabilitation studies of historic buildings;
- Protective measures for rural resources including easements and local legislation;
- historic structures and historic landscape reports;
- preservation advocacy efforts;
- special outreach and training programs for residents, crafts, and trades people, government officials, and others;
- building code compliance, accessibility and energy studies;
- historic preservation plans and design guidelines as components of broader planning efforts.

This program may be utilized to prepare a historic resources report for buildings located within the Conservation District Plan Study area boundaries.

D. NATIONAL SMALL BUSINESS ADMINISTRATION TREE PLANTING PROGRAM

The Small Business Administration's (SBA) National Small Business Tree Planting Program is administered by the New York State Department of Environmental Conservation.

The amount of funding is based on a community's size; the Village of Suffern, with a population size of 11,055, may be awarded \$5,000. The grant must be matched by a minimum 25% in local non-federal funds. Tree planting must be done by a small business, not by municipal employees. Trees planted must meet the American Standard for Nursery Stock, and be of appropriate size to ensure survival, and in accordance with a tree planting plan.

Funds should be tapped for implementing the tree plantings recommended in the Conservation District Plan.

E. NEW YORK MAIN STREET ALLIANCE

The New York Main Street Alliance, Inc. (NYMSA), is a coalition of non-profit organizations, practitioners, community leaders, and state and municipal agencies dedicated to Main Street revitalization in New York State. While this organization does not provide direct funding, it does provide technical assistance to communities that desire to strengthen their "main street."

This organization recently published a Resource Manual (August 1993) that represents a compendium of public and private sector funding sources and technical assistance providers available for Main Street revitalization in New York State.

F. NEW YORK STATE DEPT. OF TRANSPORTATION

The Transportation Enhancement Program of the Federal Intermodal Surface Transportation Efficiency Act (ISTEA) is designed to improve the appearance of transportation corridors. Transportation enhancement activities has been defined broadly to include a number of "main street" related issues including:

- historic preservation
- acquisition of scenic easements and scenic historic sites
- scenic or historic highway programs
- rehabilitation and operation of historic transportation buildings
- provision of facilities for pedestrians and bicycles
- preservation of abandoned railway corridors, including conversion of use for pedestrians and bicycles
- landscaping and other scenic beautification
- control and removal of outdoor advertising
- mitigation of roadway runoff pollution

Funds are distributed through the New York State Department of Transportation offices. Incorporated groups, municipalities, and state agencies and authorities are eligible applicants and may apply for funding directly to NYSDOT.

Funds from this program could be used to beauty the Conservation District area, and restore the former freight rail station.

G. LOCAL FUNDING

1. Suffern Parking Authority

The Suffern Parking Authority receives funds through the sale of permits for long-term parking, e.g, commuter parking, at the Village's parking lots. These funds could also be used to provide additional parking along Orange Avenue, and to generally beautify the existing parking lots through landscaping.

2. Suffern Chamber of Commerce

The Suffern Chamber of Commerce, through its street fairs and other fundraising events, continuously supports the beautification of the central business district through various programs, e.g., the replacement of signs and awnings. The Village should work cooperatively with the Suffern Chamber of Commerce to effectuate other streetscape improvements within the Conservation District Area.

3. Community Development Block Grants

The Village receives Community Development Block Grants disbursed by the county. Currently, the Village's Community Development Committee is working to obtain and use this funding for street lighting. In addition to lighting, these funds could also be used for other recommended street improvements, e.g., street trees, within the Conservation District area.

**APPENDIX A
PROPERTY AND STRUCTURES INVENTORY**

PROPERTY INVENTORY

MAP NO.	TAX MAP BLOCK/LOT	BUILDING CHARACTERISTICS							
		APPROX. DATE OF CONSTRUCTION	ROOF	STORIES	FACADE MATERIALS	DESCRIPTION			
A1	53B1	"very old"	gable	2.5	wood frame				
A2	54.76	"very old"		3	brick	former Suffern Hotel			
A3	77A, 77B					vacant			
A4	NYS					New York State Thruway right-of-way			
A5	78B, 79	1920±	flat		stucco, tile, brick				
A6	80					vacant			
A7	81	1915	flat	2	concrete block				
A8	82	1900	gable	2	vinyl	residence <i>single family</i>			
A9	6A, 83, 6A3	1901; 1930; 1960	flat	2	brick	Suffern firehouse			
A10	55					vacant			
A11	56	1900; 1954	mansard; gable	2	wood				
B1	84A	1910	flat	2	brick	flatiron building			
B2	84B	1890	flat	2	asphalt shingles	stores; garage facing onto Wayne Avenue			
B3	84C	very old	gable	2	vinyl	S&L building			
B4	85	1800	flat	1	wood	Port of Restoration			
B5	86, 87A, 87B, 88	1922	parapet	1	stucco on tile	garage; warehouse			
B6	89	"old"	gable	2	stucco and timber; asphalt siding	warehouse			
B7	90	1888	gable	1	pre-fab metal				
B8	91A	1900±	gable	2	aluminum	single family			
B9	91B1	1880±	mansard	2	asphalt shingles	single family			
B10	92	1890±	gable	2	wood shingles	single family			

BUILDING CHARACTERISTICS						
MAP NO.	TAX MAP BLOCK/LOT	APPROX. DATE OF CONSTRUCTION	ROOF	STORIES	FACADE MATERIALS	DESCRIPTION
B11	915	no date	flat	1		1968 fire - 85% destroyed - warehouse showroom
B12	107	1900±	gable	2	wood shingles	single family
B13	116	1900±	gable front	2	asphalt shingles	single family
B14	126	1900±	gable front	2		single family
B15	115B2	1930±	gable front	2		single family
B16	115A,115B1	1900±	gable		stucco	single family
B17	103B	1900±	gable	2	wood shingle	single family
B18	103A	1900±	gable front		stucco	single family
C1	106A	1980s	flat	2	brick	commercial building
C2	106B	1920±	flat	2	stucco & timber	Comesky Block
C3	123	1916	flat			Shuart Building
C4	124	1905				commercial building
C5	114	1900±	flat	2	brick front	commercial building (Celeste's Best)
C6	125	1925±				
C7	136	1900;1981	flat	1	brick front	commercial building (was cross gable; 2nd story demolished)
C8	137A	1986				
C9	142A	1986				
C10	138A2	1935	flat	1	brick & concrete	U.S. Post Office; National Register; Art Deco building
C11	138A1A					vacant
C12						rail right-of-way
D1	122					park
D2	133	1976	flat	2	brick front	commercial building (DaNina's)
D3	134					vacant
D4	135					vacant
D5	139	very old	flat	2	brick; stucco frame	commercial building (Chazz)

BUILDING CHARACTERISTICS							
MAP NO.	TAX MAP BLOCK/LOT	APPROX. DATE OF CONSTRUCTION	ROOF	STORIES	FACADE MATERIALS	DESCRIPTION	
D6	141	1957	flat; parapet	1	stucco facade	commercial building (Marcello's)	
D7	149	1949	flat;parapet	1	stucco	commercial building (Chinese rest.)	
D8	150	?	flat	1	brick	commercial building (Levine's)	
D9	151	1930±	flat	1	brick	commercial building; Colonial style (Pronto Press)	
D10	159	1908	flat	2	brick	commercial building (Hearing Aid)	
D11	168	very old	gable; flat	2	wood shingle;	storefront attached to cross gable wood frame structure (Pasquale's)	
D12	169A	1959 1935±	flat flat	1 1	stainless steel stucco	diner Chestnut Street side - former Club 21	
D13	169B	1962	flat	1	concrete block	commercial building (dry cleaners)	
D14	179	?	flat				
D15	178	1900±	flat		solid brick		
D16	177	1930±	flat		stucco	commercial building; former bowling alley; fire in 1968; renov. 1969	
D17	190	1860±	gable	2	wood shingles		
D18	189	1936	flat	1	stucco	former Texaco station	
D19	176B	1910±	gable	1	stucco	automotive	
D20	167A,167B,176A					vacant;parking lot	
D21	158	1930	flat	3	brick;stucco	former Lafayette Hotel	
D22	148B	1910±	flat		stucco		
D23	148A	1910±	gable		stucco		
D24	140B	1915±	flat	2	stucco		
D25	140A	1915±	flat	2	stucco	commercial building; fire-renov. in 1980	
E1	215	1900±					
E2	205						
E3	204					vacant	

BUILDING CHARACTERISTICS						
TAX MAP NO.	TAX MAP BLOCK/LOT	APPROX. DATE OF CONSTRUCTION	ROOF	STORIES	FACADE MATERIALS	DESCRIPTION
E4	203					vacant
E5	200	1900	gable	2	stucco	residential w/storefront
E6	191	1900,1957??				
E7	192A	1896				
E8	192B	1916,1966				
E9	192C	1916	flat	2	brick	
E10	192D	1920±				
F1	143A,143B,153,194B					Suffern parking lot
F2	152	1951	flat	1	brickfront	office building
F3	161A					vacant
F4	160,170A,170B	1957;1980	flat	1	brickfront	bank building
F5	161B	1900±				
F6	162	1900±				
F7	193A	1963				
F8	193B					vacant
F9	194A	1940±				
F10	195C	1900±				
F11	195A	1914				
F12	207A	1910				
F13	207B	1940				
F14	220A	1928±				
F15	220B	1925,1978				
F16	195B,195D	no date	gabled roof	1	stucco	commercial building (KAL)

BUILDING CHARACTERISTICS									
MAP NO.	TAX MAP BLOCK/LOT	APPROX. DATE OF CONSTRUCTION	ROOF	STORIES	FACADE MATERIALS	DESCRIPTION			
F17	180B	1920				residential w/ warehouse			
F18	163,171A, 171B,180A	1929	flat;gable	4	stucco	warehouse			
F19	154					vacant			
F20						rail right-of-way			
G1	155,164A, 164B,172, 181,182, 196,197, 202	1975;1981	flat	1		Avon Warehouse			
G2	201,209, 777A,779	1919;1975	gable		brick	synagogue			
G3	208	1928,1968							
G4	777B					vacant			
G5	210,240, 241,242, 243A	1946;1991	flat	1	stucco	office building			
H1	218	1956	flat	1	brick	drive-in building			
H2	206	1922	flat	2	cut stone	Bank of New York			
H3	219	1936	gable;hip	1&2	brickfront	bank building			
H4	231	?				drive-in building			
H5	231A1	1951	flat	1;parapet	brickfront	former Woolworths			
H6	239A	1985	flat	2	brick	Miranda Building			
H7	239B,254	no dates	flat	2	stucco and brickfront	Lafayette Theatre			
H8	283,284	1912	flat	2	brickfront	former Suffern High School; Village Hall			

BUILDING CHARACTERISTICS						
MAP NO.	TAX MAP BLOCK/LOT	APPROX. DATE OF CONSTRUCTION	ROOF	STORIES	FACADE MATERIALS	DESCRIPTION
H9	285, 286, 287, 303, 334A, 334B, 335, 336, 337					
11	52	very old	gable	2	asphalt shingles	automotive uses
12	781B					rail right-of-way; former railroad station
13						parking lot
14	781C		flat	1	brickfront	small office building
15	781A					parking lot
16						rail right-of-way

**APPENDIX B
CORNELL COOPERATIVE EXTENSION
STREET TREE INVENTORY**



August 15, 1994

Bonnie Franson
Robert Geneslaw Co.
Two Executive Boulevard
Suite 401
Suffern, NY 10901

Dear Bonnie,

On August 12, I inspected the street trees of Suffern, at your request, to assess their conditions and recommend action. Attached is the list of trees, each number to correspond with numerals on the accompanying map. Following are a few general observations.

1. **VARIETY:** Almost 60% of the trees are maples. If a maple disease or insect pest were to attack the maples, Suffern could lose a majority of its street trees. In future plantings, I would recommend that each specie account for no more than 20% of the total number of species in the village. In other words, there should be at least five different species of trees planted in the village. Cornell University specialists are even more strict, recommending at least 20 different species in a city/village street tree planting plan. Recommended trees, depending on specific location, are listed below and are described in the enclosed booklet from Dr. Bassuk.

- Princeton Upright Scholar Tree
- Hedge maple
- Amur maple
- any upright crabapples (must be resistant to apple scab disease in Rockland)
- Accolade flowering cherry
- Japanese tree lilac
- Swedish mountain ash
- columnar forms fo Norway maple, red maple
- upright (Fastigiata) European hornbeam
- upright ginkgo (eg. Princeton Sentry)
- honeylocusts
- columnar Sargent cherry
- callery pear
- willow oak

2. SHAPE: Most of the trees planted are ones that will eventually grow to have large crowns or canopies. Considering the sidewalk width, I would recommend that trees with narrow crowns be chosen; this may include columnar types and small headed trees.
3. PLACEMENT: Many tree trunks have been damaged by cars backing into them. Trees should be placed far enough from the curb so as to avoid this problem.
4. PLANTING PITS: Many trees are scorched or stunted due to the small planting pits (ie. 2' x 2' in size). These should be enlarged; see enclosed planting pit construction details as provided by Dr. Nina Bassuk of the Urban Horticulture Insitute, Cornell University. *Soil improvement is the key* to their survival.
5. GRATES: I would recommend the use of large metal grates -- a minimum of 3' x 3' in size (preferably 4' x 4') around each tree. Bricks and stones used in some cases are lifting out of the ground, creating a tripping hazard.
6. MAINTNENANCE: A number of trees would benefit from pruning this winter -- to remove dead wood. Others, as noted, would benefit from fertilizer applied in spring as the new leaves open.
7. DESIGN: My personal opinion is that a downtown area such as Suffern's can be beautifully enhanced by the planting of trees at regular intervals (ie. every 20-25") along Lafayette Avenue.
8. CODES ON MAP: On the charts under the heading "CONDITION" you will find numerals 0-4. In this simple rating system, the overall condition of the trees were judged as listed below. I would recommend that those with a 0, 1 or 2 rating be removed as part of your plan.

4 = excellent condition
3 = good condition
2 = fair condition
1 = poor condition
0 = dead

I hope this information is useful. Please feel free to call if you have further questions or concerns.

Sincerely,



Paul Trader
Cooperative Extension Agent

Suffern Street Tree Assessment
 August 12, 1994
 Paul Trader, Cornell Cooperative Extension

LOCATION	TYPE OF TREE	CONDITION*	RECOMMENDATION
1. southwest corner of Avon	sugar maple	(3) -- minor heart rot; poor pruning along utility lines	OK as is
2. northeast corner of Washington & Lafayette	Norway maple	(2) -- thinning crown; possible maple decline; heart rot, girdling roots, restricted root zone	will live estimated 5 years if water is provided; prune out dead branches
3. 46 Lafayette	Norway maple	(2) half gone due to poor utility pruning	remove; don't replant in this grass strip; plant in island nearby
4. Congregation Sons of Israel	Norway maple	(3) dying top; much of top has been cut back	leave as is until it dies in 5-10 years; prune dead/snapped top branches
5. Avon Park by memorial	honeylocust	(3) thin but healthy	OK; leave as is
6. Avon park, corner of Lafayette & Washington	Norway maple	(3) less than 10% dead branches	prune out two dead branches
7. Avon Park, south border by watering trough	Norway maple	(2-3) badly shaped due to pruning and dieback	prune out dead branches; replace when it dies in 5-10 years
8. southwest corner of Avon Park	Norway maple	(3) shape is OK; dead branches; heart rot	prune out dead branches
9. west side of Avon Park	elm	(3) OK; thin but healthy	leave as is
10A/B. property of Sons of Israel; Suffern Lane	two red maples; 50' tall each	(4) pruned bad on south side but otherwise healthy	leave as is

* (4) = excellent condition; (3) = good condition; (2) = fair condition; (1) = poor condition; (0) = dead

LOCATION	TYPE OF TREE	CONDITION*	RECOMMENDATION
11/12/13. east side of CMS Corp. along Suffern Lane	three 30' tall honeylocusts	(3) good condition; small planting pits	leave as is
14. east side of #1 Suffern Lane	Norway maple; 20' tall	(1) columnar, scorch, stunted; 2'x3' planting pit	remove/replace with larger pit and narrow tree (max. 5' radius)
15-20. front of Marine Midland bank	six honeylocusts, each 10-20' tall	(4) four are in good condition; (3) one is short with dead wood inside; (1) one is stunted	leave four as is thin out dead wood replace; improve soil
21. Balsamo/Byrne Law Office along Chestnut	sugar maple	(1) bad condition; heart rot, stunted, scorch	replace (Zelkova would be good choice)
22/23. North Fork Bank along Chestnut	two flowering cherries	(2) each 20' tall; thin crowns, dropping lvs.	one is OK; southernmost one is very thin; wait til '95 to see if it rebounds
24. southwest corner of N. Fork Bank along Lafayette	Norway maple	(2) scorch, poor pruning (lopsided) 3x3' planting pit	remove; create larger planting pit
25. #38 Lafayette	Norway maple	(1) columnar, scorch, dead branches	remove
26. #38 Lafayette	red maple	(1) scorch, stunted, much trunk damage	remove
27. north end of train lot	honeylocust	(4) good shape and size	OK as is
28. north end of train lot	Crimson King Norway maple	(4) small size; healthy	OK as is
29. SE corner of Lafayette & Orange Tpk.	Kwanzan cherry	(1) stunted; infested with White Prunicola Scale insects	remove; replant other type in center of island
30. park on corner of Lafayette & Orange Tpk.	Crimson King Norway maple	(4) excellent condition	OK as is

* (4) = excellent condition; (3) = good condition; (2) = fair condition; (1) = poor condition; (0) = dead

LOCATION	TYPE OF TREE	CONDITION*	RECOMMENDATION
31. in park on corner of Lafayette & Orange Tpk.	spruce	(3) good condition	OK as is
32. front of daNina restaurant on Lafayette	sugar maple	(2) columnar; healthy but stunted due to small planting pit	remove and plant tree with larger head; enlarge pit
33. in front of #21 Lafayette (Marcello's)	maple	(1) columnar; deformed, stunted, scorch, bad pruning	remove
34. #29 Lafayette (Pronto Press)	Norway maple	(0) dead	remove; enlarge planting pit
35. #39 Lafayette	Norway maple	(2) scorch, small head, heart rot	it will live but looks bad, aesthetically
36. #45 Lafayette	Norway maple	(3) scorch; good crown; small (2x2') planting pit	enlarge planting pit
37. #55 Lafayette (Colonial Florist)	Norway maple	(1) lopsided; half of crown is missing; 2x2' planting pit	remove; enlarge pit
38. Renato's Bakery on Lafayette	Norway maple	dead; 2x2' pit	remove; enlarge pit
39. Bank of New York; west side of building	honeylocust	(4) thin but healthy	OK as is
40. Bank of New York; west side of building	callery pear	(3) leaning; may snap in ice/snow	OK; cable to building

* (4) = excellent condition; (3) = good condition; (2) = fair condition; (1) = poor condition; (0) = dead

LOCATION	TYPE OF TREE	CONDITION*	RECOMMENDATION
41. Bank of New York; west side of building	honeylocust	(4) thin but health	OK as is
42. Bank of New York; west side of building	birch	(0) dead	remove
43. 81 Lafayette	pin oak	(1) shredded branches; scorch, stunted	tree will grow too large for area; remove
44. 81 Lafayette	pin oak	(1) shredded branches; scorch, stunted	tree will grow too large for area; remove
45. 89 Lafayette	pin oak	(3) good condition	OK as is
46. M&T bank	40' tall Norway maple	(1) stunted, scorched, dieback	will be dead in 2-4 years due to dieback
47-53. M&T bank corner of Laf. & Wash.	gingko trees	most are healthy; one is chlorotic (yellow)	all need water, fertilizer and mulch over soil to stay moist
54. Theater parking lot along Wash. St.	40' Norway maple	(2) scorch, cupped leaves, bare top, girdled roots, dead branches	may live 5+ years if watered; prune dead branches now
55. Theater parking lot along Wash. St.	40' Norway maple	(1) same as #54 above	may live 5+ years if watered; prune dead branches now
56. Theater parking lot -- in island	red maple	(3) thin but healthy	OK as is
57. Island between theater and Village Hall	Norway maple	(2) filled with poison ivy; poorly shaped due to pruning of 2 leaders	remove
58. Village Hall front lawn	sugar maple	(3) old but healthy	keep watered, fertilized & pruned
59. Village hall, along Wash. St.	sugar maple	(3) old but healthy	keep watered, fertilizer & pruned
60. Village hall, along Wash. St.	sugar maple	(3) heart rot, dieback	keep watered, fertilized, pruned

* (4) = excellent condition; (3) = good condition; (2) = fair condition; (1) = poor condition; (0) = dead

LOCATION	TYPE OF TREE	CONDITION*	RECOMMENDATION
61. Southeast corner of Village Hall lawn	silver maple	(3) healthy	OK as is
62. Sacred Heart, south end of lawn along Wash. St.	Norway maple	(1) thin and old	remove eventually
63. Sacred Heart, south end of lawn along Wash. St.	sugar maple	(1) scorch, dieback, thin	remove eventually
64. Sacred Heart, south end of lawn along Wash. St.	columnar Norway maple	(4) healthy	OK as is
65. Sacred Heart, middle of lawn along Wash. St.	sugar maple	(3) chlorotic, heart rot, weak branch crotch	fertilize, strengthen cabling
66. Sacred Heart, middle of lawn along Wash. St.	sugar maple	(3) chlorotic, leaves otherwise healthy	fertilize
67. North end of Sacred Heart	columnar Norway maple	(4) healthy	OK as is
68. North end of Sacred Heart	columnar Norway maple	(4) healthy	OK as is
69. Northwest corner of Sacred Heart	sugar maple	(3) relatively healthy; evidence of scorch, chlorotic, dieback in top	fertilize, water lawn/root zone
70. Northwest corner of Sacred Heart	sugar maple	(3) relatively healthy; evidence of scorch, chlorotic, dieback in top	fertilize, water lawn/root zone
71. Sacred Heart near driveway along Lafayette	sugar maple	(3) cabled; color good, dieback minimal	OK; prune and water
72. Sacred Heart, near telephone pole along Lafayette	Norway maple	(4) new 30" tree	OK as is
73. In front of McDougall & Drumm, Lafayette	Sugar maple	(3) dieback, discoloration; half of canopy pruned off by utility	remove

* (4) = excellent condition; (3) = good condition; (2) = fair condition; (1) = poor condition; (0) = dead

APPENDIX C
NYSDOT PRELIMINARY PLAN FOR IMPROVEMENTS TO
WAYNE AVE./ORANGE AVE. INTERSECTION